

**Joint Meeting of Public
Works & Public Utilities
Committees**

Tuesday, April 13, 2021

6:00 PM

**McFarland Municipal Center
Community Room**

AGENDA

You are invited to this meeting through a Zoom webinar. The Public is strongly encouraged to watch and participate in these meetings remotely through either the webinar or telephone options listed below.

PLEASE CLICK THE LINK BELOW TO JOIN THE ZOOM WEBINAR:

<https://us02web.zoom.us/j/85002447707>

Or by Telephone: +1 (312) 626-6799

Webinar ID: 850 0244 7707

1. CALL TO ORDER, ROLL CALL.
2. PUBLIC APPEARANCES.
 - a. This is an opportunity for members of the public to address the Village Board. Please remember this is a virtual meeting conducted through the Zoom online meeting platform. Zoom meeting attendees wishing to address the board may do so using the Question and Answer feature within the Zoom online meeting platform. You may state your name, address, and provide your comments to the board for their consideration. Members of the public who are present in person and wish to address the board should fill out a public comment form and turn into the meeting chairperson. Members of the public may speak during public appearances or during their selected agenda item as they designate on the public comment form. Please adhere to the 3-minute time limit. Additionally, you may send your public comments to aimée.irwin@mcfarland.wi.us to be included as part of the meeting.
3. BUSINESS.
 - a. Discussion and action to make a recommendation to the Village Board regarding the award of contract for the 2021 Street Paving and Stormwater Projects.
 - b. Discussion and action to make a recommendation to the Village Board regarding 2021 Crackseal and Chipseal proposal for street maintenance.
 - c. Update and discussion regarding GIS Development.
 - d. Presentation of the Public Works Monthly Report
4. SCHEDULE NEXT MEETING DATE.
 - a. Public Works Committee: Tuesday, May 11, 2021 at 6:00 p.m.

b. Public Utilities Committee: Tuesday, May 18, 2021 at 6:00 p.m.

5. ADJOURNMENT.

This meeting notice constitutes an official meeting of the above referenced group and was posted in accordance with all applicable laws related to Open Meetings Law. It is possible that members of and possibly a quorum of members of other governmental bodies of the municipality may be in attendance at the above stated meeting to gather information. No action will be taken by any governmental body at the above stated meeting other than the governmental body specifically referred to above in this notice. Upon reasonable notice, efforts will be made to accommodate the needs of disabled individuals. For additional information or to request this service, contact the McFarland Municipal Center at (608) 838-3153 or cassandra.suettinger@mcfarland.wi.us.


VILLAGE OF
McFarland
SUMMARY SHEET

MEETING DATE: Tuesday, April 13, 2021

SECTION: Business

DEPARTMENT: Public Works

CONTACT: Jim Hessling, Public Works Director

AGENDA ITEM: Discussion and action to make a recommendation to the Village Board regarding the award of contract for the 2021 Street Paving and Stormwater Projects.

PREVIOUS ACTION:

This topic was addressed and approved at the March 9, 2021 Public Works committee and the March 16, 2021 Public Utilities meetings.

ISSUE SUMMARY:

As part of the current plan set for the 2021 Street and Storm Sewers Project, this project will encompass a handful of smaller projects into one bid package to attract more bidders and improve construction pricing. The work includes pulverizing/resurfacing Renee Court from Lewis Lane to Exchange Street, multiple large pavement patches throughout the Village, improvements to the pedestrian bridge/culvert south of Larson Beach Road, and storm sewer repairs on Lake Edge Road and in the Lani Lane/Pheasant Run area. We are requesting Committee approval to send this on to the Village Board for final approval.

We structured the bid so that we can perform additional patching around the village on various streets on a per quantity unit price. Town & Country Engineering has provided an analysis of the BIDS received included within the packet. Town & Country Engineering recommends the award of contract to Raymond P. Cattell, Inc., in the amount of \$263,185.10. This amount includes the upgrade to a Reinforced Concrete Pipe from a Corrugated Metal Pipe.

FINANCIAL/BUDGET IMPACT:

Total costs for the project are as follows:

261,610.10	Base Bid
1,575.00	Alternate Bid *
10,536.69	Engineering
25,000.00	Inspection
29,872.18	Contingency (10%)
(93.97)	Rounding
<hr/>	
328,500.00	Total Expense

* The full alternative within the award letter is presented at \$13,125. However, the alternate bid replaces entirely an item within the base bid. The cost is not additive to the total when the



alternate is selected. You have to remove the entire cost of that item from the base bid which was \$11,550 and replace it with the entire cost of the alternate which was \$13,125. The net effect is the cost of the project increases by \$1,575.

Total funding sources for the project are as follows:

300,000.00	Street Maintenance (Capital Projects Fund 400)
<u>86,750.00</u>	<u>Stormwater Projects (Capital Projects Fund 400)</u>
386,750.00	Total Revenues

The project as bid with the alternates as recommended remains under budget by approximately \$58,250.

VILLAGE PLAN REFERENCE:

None.

ORDINANCE REFERENCE:

None.

BOARD, COMMISSION OR COMMITTEE RECOMMENDATION:

Recommendation to the Village Board should be to award contract to Raymond P. Cattell, Inc. of Madison in an amount of \$263,185.10 including the base bid plus alternate and approving total project costs of \$328,500 which includes a 10% contingency.

ATTACHMENTS:

1. Recommendation Ltr- on letterhead
2. MC 181 CURRENT SET

April 8, 2021

Village of McFarland
5915 Milwaukee Street
McFarland, WI 53558

Attention: Mr. Matthew Schuenke, Village Administrator

Subject: Analysis of Bids and Recommendation for Award of Contracts; 2021 Stormwater and Street Improvements; Village of McFarland

Bid Deadline: April 8, 2021 at 10:00 a.m. local time

Ladies and Gentlemen:

The purpose of this letter is to analyze the bids received for the 2021 Stormwater and Street Improvements project and to recommend award of a contract. This project involves pavement reconstruction on Renee Court, inlet replacements at the intersection of Land Lane and Pheasant Run Road, replacement of failing CMP pipe and pedestrian crossing at the Taylor-Farwell drainage swale, and mill and overlay road repairs at various locations, as well as an alternate bid for new 53" x 83" RCP storm sewer for the Taylor-Farwell drainage swale.

The pre-bid estimate for the base bid was \$274,641.75. Eighteen general contractors, subcontractors, and material suppliers requested sets of the plans, specifications and bidding documents. Three contractors submitted bids.

A summary of the bids is as follows:

Contractor	Base Bid	Alternate Bid
Raymond P. Cattell, Inc.	\$261,610.10	\$13,125.00
Wolf Paving	\$272,054.65	\$21,135.45
G-Pro Excavating LLC	\$304,884.35	\$17,115.00

All of the bids were properly submitted.

The low bidder, using the base bid only, or the base bid and alternate bid is Raymond P. Cattell, Inc. of Madison, Wisconsin, an experienced utility and street contractor that has completed projects for the Village of McFarland successfully in the past. Their most recent project as a prime contractor in the Village was the 2018 Street Improvements project on CTH MN/Farwell Street. More recently, they were a grading subcontractor on the 2020 Street and Utility Improvements project. The bid prices were near the original estimate for the project.

The alternate bid included for this project includes pricing for the use of 53"x83" helical elliptical RCP at the Taylor-Farwell drainage ditch. The base bid for the project includes pricing for the use of 57"x83" CMP arch pipe at this some location. If the alternate bid is accepted, the project documents state that the alternate bid item will replace the corresponding base bid item. We recommend that Raymond P. Cattell, Inc. be awarded a contract for the alternate-adjusted base bid, plus the alternate bid, if budgets allow, for a total of \$263,185.10.

This will be a unit price contract. That is, the contractor will be paid for the work actually performed on the basis on the unit prices bid. This means that the final line item costs could be either greater than or less than the bid totals. Also, unexpected conditions are sometimes encountered which result in increased project costs. Therefore, it would be wise to continue to carry the recommended 10% contingency within the Village budget.

If you have any questions with respect to our thoughts on this matter, I am available at your convenience to discuss them with you.

Very truly yours,
TOWN & COUNTRY ENGINEERING, INC.

Tim Stieve, P.E., ENV-SP
Project Engineer

TS:sai
J:\JOB#S\McFarland\MC-181-M4 2021 Street and Storm Sewer Improvements\Bidding\Recommendation Ltr.docx

BID TABULATION

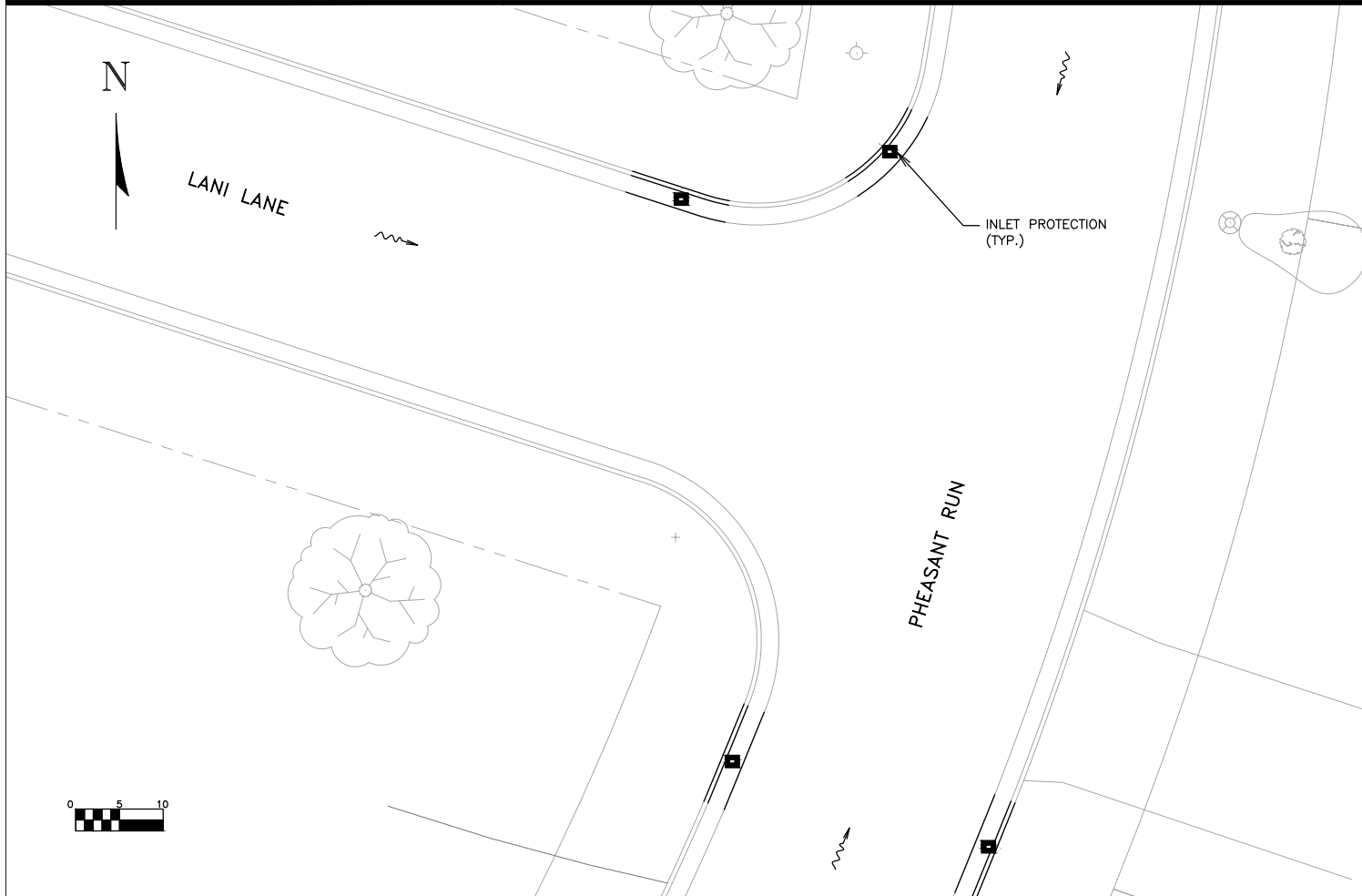
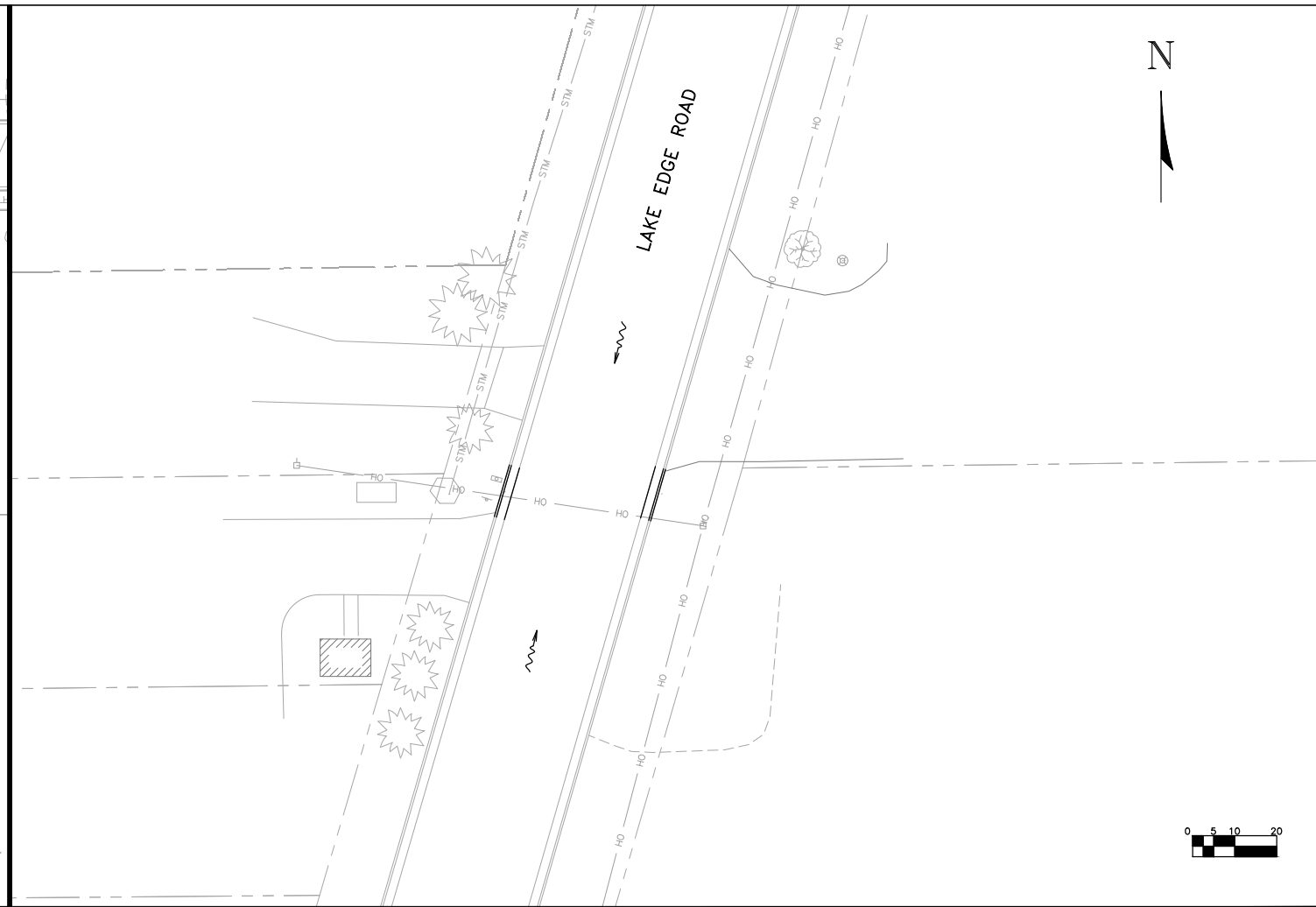
Project: 2021 Stormwater and Street Improvements; Village of McFarland
 Engineer's Project Number: MC 181 Bid Deadline: April 8, 2021 at 10:00 a.m. local time

ITEM NO.	DESCRIPTION OF WORK	BID		PRE-BID ESTIMATE		Raymond P. Cattell Inc.		Wolf Paving		G-Pro Excavating LLC	
		QUANT.	UNITS	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
BASE BID- Renee Court											
1.1	Pulverizing and Grading of Existing Asphalt Pavement	3,505	sq. yd.	\$ 3.50	\$ 12,267.50	\$ 5.00	\$ 17,525.00	\$ 2.98	\$ 10,444.90	\$ 3.80	\$ 13,319.00
1.2	0.75" Crushed Aggregate Base Course	50	tons	\$ 20.00	\$ 1,000.00	\$ 12.00	\$ 600.00	\$ 18.00	\$ 900.00	\$ 25.00	\$ 1,250.00
1.3	Excavation and Disposal of Bad Subbase Below Subgrade	1,009	cu. yd.	\$ 15.00	\$ 15,135.00	\$ 14.00	\$ 14,126.00	\$ 14.00	\$ 14,126.00	\$ 20.00	\$ 20,180.00
1.4	3" Breaker Run Replacement of Excavation of Bad Subbase Below Subgrade	2,023	tons	\$ 20.00	\$ 40,460.00	\$ 8.50	\$ 17,195.50	\$ 13.00	\$ 26,299.00	\$ 16.00	\$ 32,368.00
1.5	Sanitary Manhole Castings	1	each	\$ 700.00	\$ 700.00	\$ 600.00	\$ 600.00	\$ 965.00	\$ 965.00	\$ 650.00	\$ 650.00
1.6	New Carlson Asphalt Curb	1,955	lin. ft.	\$ 2.00	\$ 3,910.00	\$ 1.85	\$ 3,616.75	\$ 0.50	\$ 977.50	\$ 2.00	\$ 3,910.00
1.7	1.5" Hot-Mix Asphalt Surface Course, Type 5 LT	332	tons	\$ 70.00	\$ 23,240.00	\$ 68.50	\$ 22,742.00	\$ 65.00	\$ 21,580.00	\$ 69.00	\$ 22,908.00
1.8	1.75" Hot-Mix Asphalt Lower Course, Type 4 LT	388	tons	\$ 75.00	\$ 29,100.00	\$ 67.00	\$ 25,996.00	\$ 62.50	\$ 24,250.00	\$ 68.00	\$ 26,384.00
1.9	Clean & Tack	3,499	sq. yd.	\$ 0.50	\$ 1,749.50	\$ 0.15	\$ 524.85	\$ 0.30	\$ 1,049.70	\$ 0.15	\$ 524.85
1.10	2" Hot-Mix Asphalt Paving and Driveways	22	sq. yd.	\$ 30.00	\$ 660.00	\$ 17.00	\$ 374.00	\$ 40.00	\$ 880.00	\$ 18.00	\$ 396.00
1.11	Removal and Replace 5" Thick Concrete Sidewalk and Driveway Approaches	902	sq. ft.	\$ 10.00	\$ 9,020.00	\$ 12.00	\$ 10,824.00	\$ 14.00	\$ 12,628.00	\$ 10.00	\$ 9,020.00
1.12	Replace Top Section of Water Valve Boxes	1	each	\$ 750.00	\$ 750.00	\$ 200.00	\$ 200.00	\$ 660.00	\$ 660.00	\$ 500.00	\$ 500.00
1.13	Sawcutting Existing Concrete and Asphalt Pavements	260	lin. ft.	\$ 4.00	\$ 1,040.00	\$ 2.00	\$ 520.00	\$ 3.00	\$ 780.00	\$ 3.00	\$ 780.00
1.14	Topsoil Restoration, Seeding, Fertilizing & Mulching	653	sq. yd.	\$ 6.00	\$ 3,918.00	\$ 10.00	\$ 6,530.00	\$ 11.80	\$ 7,705.40	\$ 15.00	\$ 9,795.00
1.15	Traffic Control	1	lump sum	\$ 2,500.00	\$ 2,500.00	\$ 5,600.00	\$ 5,600.00	\$ 1,350.00	\$ 1,350.00	\$ 3,500.00	\$ 3,500.00
1.16	Erosion Control	1	lump sum	\$ 2,500.00	\$ 2,500.00	\$ 2,600.00	\$ 2,600.00	\$ 400.00	\$ 400.00	\$ 1,500.00	\$ 1,500.00
BASE BID- Lake Edge Road Storm Sewer											
2.1	New 24"x38" HE RCP Storm Sewer	63	lin. ft.	\$ 112.00	\$ 7,056.00	\$ 150.00	\$ 9,450.00	\$ 234.35	\$ 14,764.05	\$ 368.00	\$ 23,184.00
2.2	New 24"x38" HE RCP Storm Sewer Endwall with Gate	1	each	\$ 1,500.00	\$ 1,500.00	\$ 2,000.00	\$ 2,000.00	\$ 3,350.00	\$ 3,350.00	\$ 2,990.00	\$ 2,990.00
2.3	New Storm Saddle Inlet	2	each	\$ 1,500.00	\$ 3,000.00	\$ 2,000.00	\$ 4,000.00	\$ 1,930.00	\$ 3,860.00	\$ 2,000.00	\$ 4,000.00
2.4	Remove and Replace 30" Concrete Curb & Gutter (< 50-foot sections)	30	lin. ft.	\$ 42.00	\$ 1,260.00	\$ 50.00	\$ 1,500.00	\$ 50.00	\$ 1,500.00	\$ 40.00	\$ 1,200.00
2.5	Remove and Replace 3" Asphalt Surface Pavement, Type 5 LT	56	sq. yd.	\$ 35.00	\$ 1,960.00	\$ 60.00	\$ 3,360.00	\$ 65.00	\$ 3,640.00	\$ 50.00	\$ 2,800.00
2.6	Sawcutting Existing Concrete and Asphalt Pavements	72	lin. ft.	\$ 3.50	\$ 252.00	\$ 2.00	\$ 144.00	\$ 3.00	\$ 216.00	\$ 3.00	\$ 216.00
2.7	Topsoil Restoration, Seeding, Fertilizing & Mulching	44	sq. yd.	\$ 7.50	\$ 330.00	\$ 12.00	\$ 528.00	\$ 11.80	\$ 519.20	\$ 15.00	\$ 660.00
2.8	Traffic Control	1	lump sum	\$ 5,000.00	\$ 5,000.00	\$ 950.00	\$ 950.00	\$ 600.00	\$ 600.00	\$ 3,000.00	\$ 3,000.00
2.9	Erosion Control	1	lump sum	\$ 5,000.00	\$ 5,000.00	\$ 950.00	\$ 950.00	\$ 800.00	\$ 800.00	\$ 1,000.00	\$ 1,000.00

BID TABULATION

Project: 2021 Stormwater and Street Improvements; Village of McFarland
 Engineer's Project Number: MC 181 Bid Deadline: April 8, 2021 at 10:00 a.m. local time

ITEM NO.	DESCRIPTION OF WORK	BID		PRE-BID ESTIMATE		Raymond P. Cattell Inc.		Wolf Paving		G-Pro Excavating LLC	
		QUANT.	UNITS	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
BASE BID- Lani and Phesant Inlet Replacements											
3.1	Remove and Replace Storm Catch Basin Inlet	4	each	\$ 3,500.00	\$ 14,000.00	\$ 2,750.00	\$ 11,000.00	\$ 4,655.00	\$ 18,620.00	\$ 3,317.00	\$ 13,268.00
3.2	Remove and Replace 30" Concrete Curb & Gutter (< 50-foot sections)	48	lin.ft.	\$ 42.00	\$ 2,016.00	\$ 50.00	\$ 2,400.00	\$ 50.00	\$ 2,400.00	\$ 40.00	\$ 1,920.00
3.3	Sawcutting Existing Concrete and Asphalt Pavements	126	lin. ft.	\$ 3.50	\$ 441.00	\$ 2.00	\$ 252.00	\$ 3.00	\$ 378.00	\$ 3.00	\$ 378.00
3.4	Topsoil Restoration, Seeding, Fertilizing & Mulching	89	sq. yd.	\$ 7.50	\$ 667.50	\$ 12.00	\$ 1,068.00	\$ 11.80	\$ 1,050.20	\$ 15.00	\$ 1,335.00
3.5	Remove and Replace 3" Asphalt Surface Pavement, Type 5 LT	55	sq. yd.	\$ 35.00	\$ 1,925.00	\$ 60.00	\$ 3,300.00	\$ 65.00	\$ 3,575.00	\$ 50.00	\$ 2,750.00
3.6	Traffic Control	1	lump sum	\$ 2,000.00	\$ 2,000.00	\$ 950.00	\$ 950.00	\$ 650.00	\$ 650.00	\$ 2,500.00	\$ 2,500.00
3.7	Erosion Control	1	lump sum	\$ 2,000.00	\$ 2,000.00	\$ 950.00	\$ 950.00	\$ 2,400.00	\$ 2,400.00	\$ 1,500.00	\$ 1,500.00
BASE BID- Taylor-Farwell Drainage Swale											
4.1	New 83"x57" CMP Storm Sewer	21	lin. ft.	\$ 300.00	\$ 6,300.00	\$ 550.00	\$ 11,550.00	\$ 865.35	\$ 18,172.35	\$ 721.00	\$ 15,141.00
4.2	Remove and Replace 5" Thick Concrete Sidewalk	138	sq. ft.	\$ 10.00	\$ 1,380.00	\$ 12.00	\$ 1,656.00	\$ 24.35	\$ 3,360.30	\$ 10.00	\$ 1,380.00
4.3	Remove and Replace Treated Wood Railing	78	lin. ft.	\$ 35.00	\$ 2,730.00	\$ 50.00	\$ 3,900.00	\$ 45.00	\$ 3,510.00	\$ 45.00	\$ 3,510.00
4.4	Topsoil Restoration, Seeding, Fertilizing & Mulching	102	sq. yd.	\$ 7.50	\$ 765.00	\$ 12.00	\$ 1,224.00	\$ 11.80	\$ 1,203.60	\$ 15.00	\$ 1,530.00
4.5	Erosion Control Matting	65	sq. yd.	\$ 20.00	\$ 1,300.00	\$ 5.00	\$ 325.00	\$ 4.00	\$ 260.00	\$ 8.00	\$ 520.00
4.6	Erosion Control	1	lump sum	\$ 4,000.00	\$ 4,000.00	\$ 2,000.00	\$ 2,000.00	\$ 3,000.00	\$ 3,000.00	\$ 1,500.00	\$ 1,500.00
BASE BID- Miscellaneous Mill and Overlay											
5.1	2" Mill and Overlay	4,849	sq. yd.	\$ 3.25	\$ 15,759.25	\$ 3.00	\$ 14,547.00	\$ 3.25	\$ 15,759.25	\$ 3.50	\$ 16,971.50
5.2	2" Hot-Mix Asphalt Surface Pavement, Type 5 LT	614	tons	\$ 75.00	\$ 46,050.00	\$ 88.00	\$ 54,032.00	\$ 70.80	\$ 43,471.20	\$ 89.00	\$ 54,646.00
BASE BID TOTAL					\$ 274,641.75		\$ 261,610.10		\$ 272,054.65		\$ 304,884.35
ALTERNATE BID - Taylor-Farwell Drainage Swale											
A1.1	New 53"x83" RCP Storm Sewer	21	lin. ft.	\$ 750.00	\$ 15,750.00	\$ 625.00	\$ 13,125.00	\$ 1,006.45	\$ 21,135.45	\$ 815.00	\$ 17,115.00



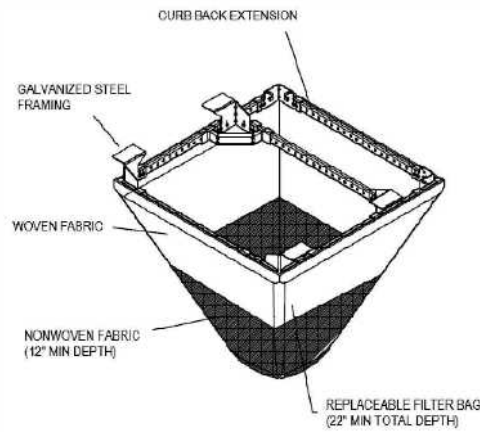
2912 Marketplace Drive
Suite 103
Madison, WI 53719
(608) 273-3350
www.tceengineers.net

tc TOWN & COUNTRY
ENGINEERING, INC.

EROSION CONTROL PLAN

2021 STREET AND STORM SEWER IMPROVEMENTS
Renee Court, Pheasant Run, Lake Edge Road,
Taylor-Farwell Storm Sewer
Village of McFarland, Wisconsin

PROJECT NO.:	MC 181
DRAWING FILE:	MC 181 SHEETS.DWG
DRAWN BY:	J.C.A.
CHECKED BY:	T.J.S.
DATE:	2-17-21
REVISIONS:	
SCALE:	VARIES
SHEET:	1



GENERAL NOTES:

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED IN THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

FRAMED INLET PROTECTION SHALL BE COMPLIANT WITH ALL ASTM STANDARD D8057-17 REQUIREMENTS, INCLUDING:
 A. BYPASS OVERFLOW THAT MEETS OR EXCEEDS INLET DESIGN FLOW
 B. FRAME AND BAG STRONG ENOUGH TO HANDLE FULL SEDIMENT LOAD.

INSTALLATION NOTES:

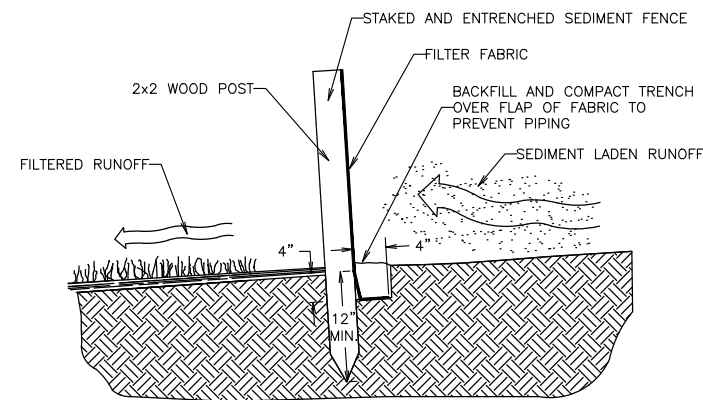
NO PART OF INLET PROTECTION SHALL BE PROJECTING ABOVE THE GRATE.

FOR COMBINATION INLETS, PROTECTION SHALL CAPTURE RUNOFF ENTERING BOTH GRATE AND CURB OPENING.

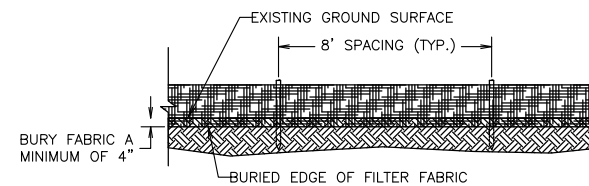
A DUAL FABRIC FILTER BAG, WITH NON-WOVEN BOTTOM AND WOVEN TOP SHALL BE USED.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

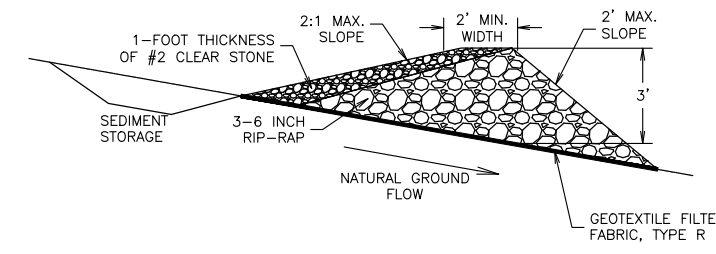
DETAIL
INLET PROTECTION - FRAMED (W/ CURB BOX)



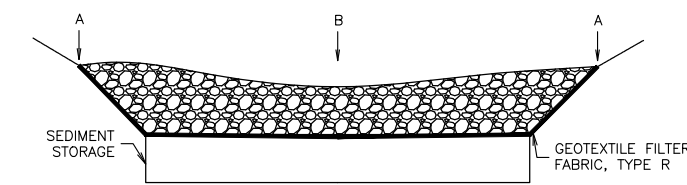
CROSS-SECTION OF A PROPERLY INSTALLED SEDIMENT FENCE



DETAIL
SEDIMENT FENCE



SECTION VIEW



FRONT VIEW

DETAIL
STONE CHECK DAM

EROSION CONTROL NOTES:

- LOCATIONS MARKED WITH "■" TO RECEIVE INLET FILTER PROTECTION DURING CONSTRUCTION. ALL NEW STREET INLETS MUST ALSO RECEIVE INLET FILTER PROTECTION.
- CONSTRUCT A STONE CHECK DAM IN GUTTER LINE AT ALL LOCATIONS MARKED WITH "▲"
- SURFACE FLOW DIRECTION IS INDICATED WITH
- SILT FENCE INSTALLATION IS INDICATED WITH
- KEEP A COPY OF THE CURRENT EROSION CONTROL PLAN ON SITE THROUGHOUT THE DURATION OF THE PROJECT.
- SUBMIT PLAN REVISIONS OR AMENDMENTS TO THE WDNR AT LEAST 5 DAYS PRIOR TO FIELD IMPLEMENTATION.
- THE CONTRACTOR IS RESPONSIBLE FOR ROUTINE SITE INSPECTIONS AT LEAST ONCE EVERY 7 DAYS AND WITHIN 24 HOURS AFTER A RAINFALL EVENT OF 0.5 INCHES OR GREATER. KEEP INSPECTION REPORTS ON-SITE AND MAKE THEM AVAILABLE UPON REQUEST.
- INSPECT AND MAINTAIN ALL INSTALLED EROSION CONTROL PRACTICES UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED.
- WHEN POSSIBLE: PRESERVE EXISTING VEGETATION (ESPECIALLY ADJACENT TO SURFACE WATERS), MINIMIZE LAND-DISTURBING CONSTRUCTION ACTIVITY ON SLOPES OF 20% OR MORE, MINIMIZE SOIL COMPACTION, AND PRESERVE TOPSOIL.
- REFER TO THE WDNR STORMWATER CONSTRUCTION TECHNICAL STANDARDS AT http://dnr.wi.gov/topic/stormwater/standards/const_standards.html.
- INSTALL PERIMETER EROSION CONTROLS AND ROCK TRACKING PAD CONSTRUCTION ENTRANCE(S) PRIOR TO ANY LAND-DISTURBING ACTIVITIES, INCLUDING CLEARING AND GRUBBING. USE WDNR TECHNICAL STANDARD STONE TRACKING PAD AND TIRE WASHING #1057 FOR ROCK CONSTRUCTION ENTRANCE(S).
- INSTALL INLET PROTECTION PRIOR TO LAND-DISTURBING ACTIVITIES IN THE CONTRIBUTING DRAINAGE AREA AND/OR IMMEDIATELY UPON INLET INSTALLATION. COMPLY WITH WDNR TECHNICAL STANDARD STORM DRAIN INLET PROTECTION FOR CONSTRUCTION SITES #1060.
- STAGE CONSTRUCTION GRADING ACTIVITIES TO MINIMIZE THE CUMULATIVE EXPOSED AREA. CONDUCT TEMPORARY GRADING FOR EROSION CONTROL PER WDNR TECHNICAL STANDARD TEMPORARY GRADING PRACTICES FOR EROSION CONTROL #1067.
- NOTIFY THE OWNER IF DEWATERING IS SCHEDULED TO OCCUR IN AREAS OF SOIL AND/OR GROUNDWATER CONTAMINATION, OR IF DEWATERING WILL OCCUR FROM A HIGH CAPACITY WELL (70 GPM OR MORE). DEWATER ONLY AFTER THE APPROPRIATE WDNR DEWATERING DISCHARGE PERMIT HAS BEEN OBTAINED.
- PROVIDE ANTI-SCOUR PROTECTION AND MAINTAIN NON-EROSIVE FLOW DURING DEWATERING. LIMIT PUMPING RATES TO EITHER (A) THE SEDIMENT BASIN/TRAP DESIGN DISCHARGE RATE, OR (B) THE BASIN DESIGN RELEASE RATE WITH THE CORRECTLY-FITTED HOSE AND GEOTEXTILE FILTER BAG. PERFORM DEWATERING OF ACCUMULATED SURFACE RUNOFF IN ACCORDANCE WITH WDNR TECHNICAL STANDARD DE-WATERING #1061.
- INSTALL AND MAINTAIN SILT FENCING PER WDNR TECHNICAL STANDARD SILT FENCE #1056. REMOVE SEDIMENT FROM BEHIND SILT FENCES AND SEDIMENT BARRIERS BEFORE SEDIMENT REACHES A DEPTH THAT IS EQUAL TO ONE-HALF OF THE FENCE AND/OR BARRIER HEIGHT.

- REPAIR BREAKS AND GAPS IN SILT FENCES AND BARRIERS IMMEDIATELY. REPLACE DECOMPOSING STRAW BALES (TYPICAL BALE LIFE IS 3 MONTHS). LOCATE, INSTALL, AND MAINTAIN STRAW BALES PER WDNR TECHNICAL STANDARD DITCH CHECKS #1062.
- INSTALL AND MAINTAIN FILTER SOCKS IN ACCORDANCE WITH WDNR TECHNICAL STANDARD INTERIM MANUFACTURED PERIMETER CONTROL AND SLOPE INTERRUPTION PRODUCTS #1071.
- IMMEDIATELY STABILIZE STOCKPILES AND SURROUND STOCKPILES AS NEEDED WITH SILT FENCE OR OTHER PERIMETER CONTROL IF STOCKPILES WILL REMAIN INACTIVE FOR 7 DAYS OR LONGER.
- IMMEDIATELY STABILIZE ALL DISTURBED AREAS THAT WILL REMAIN INACTIVE FOR 14 DAYS OR LONGER. BETWEEN SEPTEMBER 15 AND OCTOBER 15: STABILIZE WITH MULCH, TACKIFIER, AND A PERENNIAL SEED MIXED WITH WINTER WHEAT, ANNUAL OATS, OR ANNUAL RYE, AS APPROPRIATE FOR REGION AND SOIL TYPE. OCTOBER 15 THROUGH COLD WEATHER: STABILIZE WITH A POLYMER AND DORMANT SEED MIX, AS APPROPRIATE FOR REGION AND SOIL TYPE.
- STABILIZE AREAS OF FINAL GRADING WITHIN 7 DAYS OF REACHING FINAL GRADE.
- SWEEP/CLEAN UP ALL SEDIMENT/TRASH THAT MOVES OFF-SITE DUE TO CONSTRUCTION ACTIVITY OR STORM EVENTS BEFORE THE END OF THE SAME WORKDAY OR AS DIRECTED BY THE OWNER. SEPARATE SWEEP MATERIALS (SOILS AND TRASH) AND DISPOSE OF APPROPRIATELY.
- THE CONTRACTOR IS RESPONSIBLE FOR CONTROLLING DUST PER WDNR TECHNICAL STANDARD DUST CONTROL ON CONSTRUCTION SITES #1068.
- PROPERLY DISPOSE OF ALL WASTE AND UNUSED BUILDING MATERIALS (INCLUDING GARBAGE, DEBRIS, CLEANING WASTES, OR OTHER CONSTRUCTION MATERIALS) AND DO NOT ALLOW THESE MATERIALS TO BE CARRIED BY RUNOFF INTO THE RECEIVING CHANNEL.
- COORDINATE WITH THE OWNER TO UPDATE THE LAND DISTURBANCE PERMIT TO INDICATE THE ANTICIPATED OR LIKELY DISPOSAL LOCATIONS FOR ANY EXCAVATED SOILS OR CONSTRUCTION DEBRIS THAT WILL BE HAULED OFF-SITE FOR DISPOSAL. THE DEPOSITED OR STOCKPILED MATERIAL NEEDS TO INCLUDE PERIMETER SEDIMENT CONTROL MEASURES (SUCH AS SILT FENCE, HAY BALES, FILTER SOCKS, OR COMPACTED EARTHEN BERMS).
- FOR NON-CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED SLOPES, PROVIDE CLASS I, II OR III TYPE A EROSION CONTROL MATTING. SELECT EROSION MATTING FROM APPROPRIATE MATRIX IN WDOT'S WIDOT PRODUCT ACCEPTABILITY LIST (PAL); INSTALL AND MAINTAIN PER WDNR TECHNICAL STANDARD NON-CHANNEL EROSION MAT #1052.
- FOR CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED AREAS, PROVIDE CLASS I, II, OR III TYPE B EROSION CONTROL MATTING. SELECT EROSION MATTING FROM APPROPRIATE MATRIX IN WDOT'S WIDOT PRODUCT ACCEPTABILITY LIST (PAL); INSTALL AND MAINTAIN PER WDNR TECHNICAL STANDARD CHANNEL EROSION MAT #1053.
- MAKE PROVISIONS FOR WATERING DURING THE FIRST 8 WEEKS FOLLOWING SEEDING OR PLANTING OF DISTURBED AREAS WHENEVER MORE THAN 7 CONSECUTIVE DAYS OF DRY WEATHER OCCUR.
- INSTALL ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES (SUCH AS TEMPORARY SEDIMENT BASINS, DITCH CHECKS, EROSION CONTROL MATTING, SILT FENCING, FILTER SOCKS, WATTLES, SWALES, ETC.), OR AS DIRECTED BY THE OWNER.
- THE CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE WDNR REMEDIATION AND WASTE MANAGEMENT REQUIREMENTS FOR HANDLING AND DISPOSING OF CONTAMINATED MATERIALS. SITE-SPECIFIC INFORMATION FOR AREAS WITH KNOWN OR SUSPECTED SOIL AND/OR GROUNDWATER CONTAMINATION CAN BE FOUND ON WDNR'S BUREAU OF REMEDIATION AND REDEVELOPMENT TRACKING SYSTEM (BRRTS) PUBLIC DATABASE AT: <http://dnr.wi.gov/botw/>

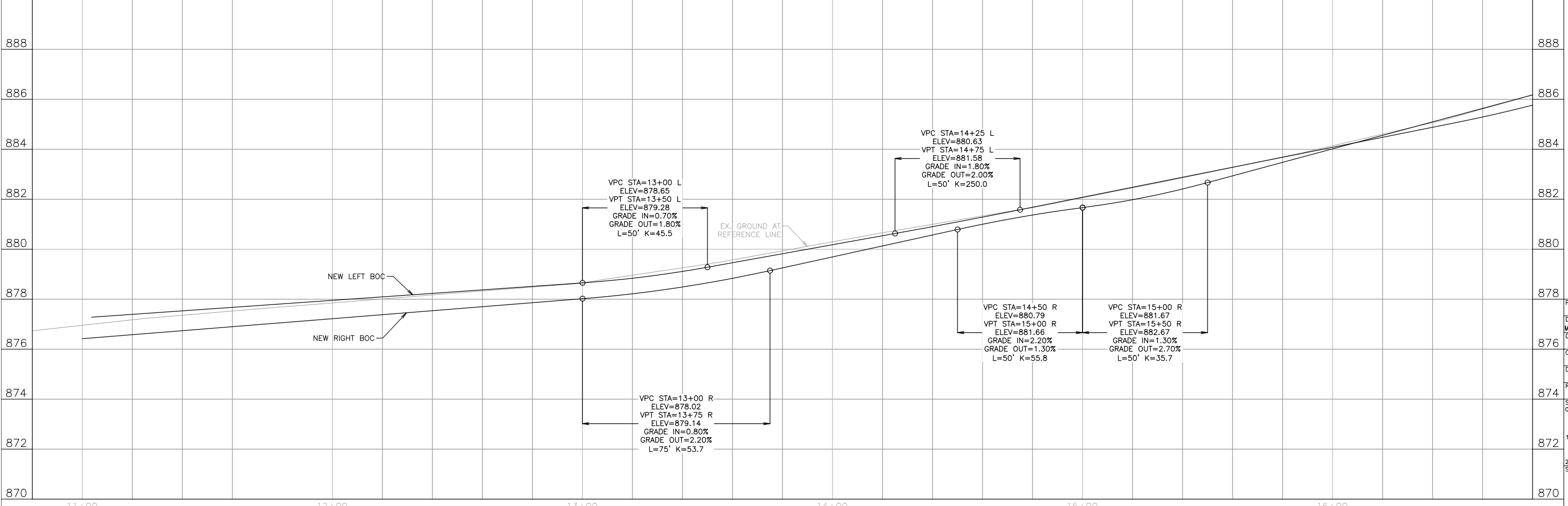
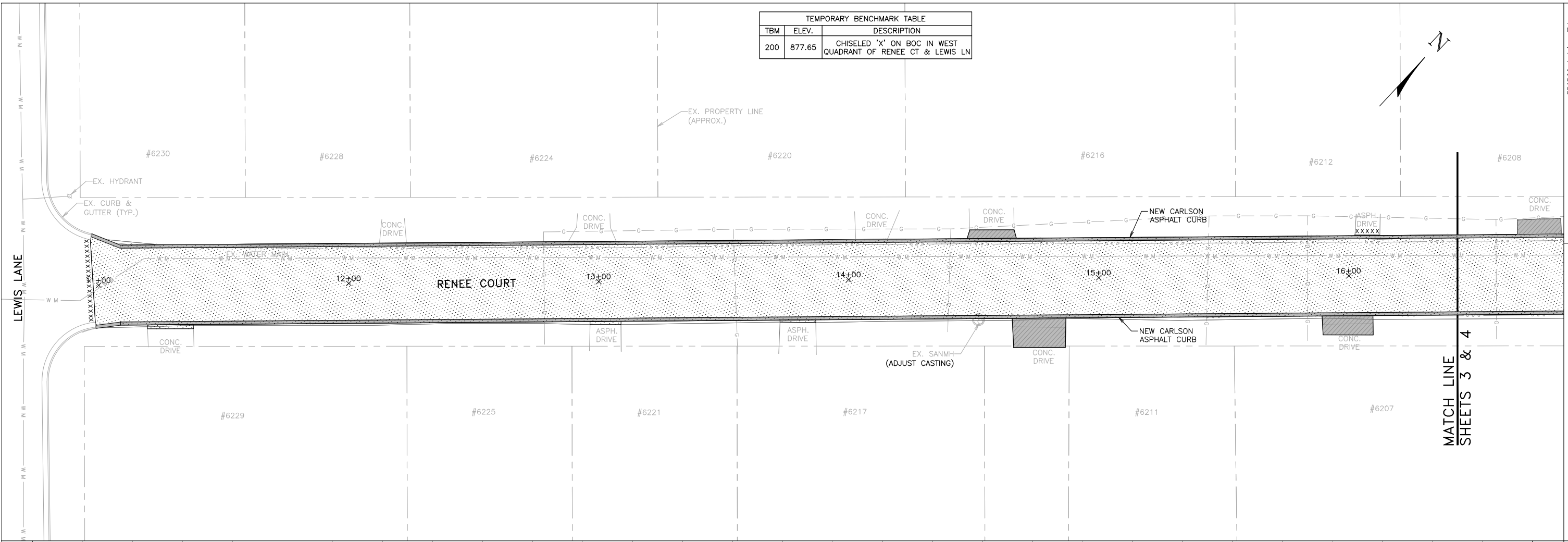
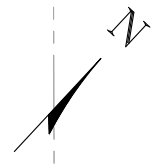
2912 Marketplace Drive
Suite 103
Madison, WI 53719
(608) 273-3350
www.tceengineers.net

TOWN & COUNTRY ENGINEERING, INC.
EROSION CONTROL NOTES & STANDARD CONSTRUCTION DETAILS

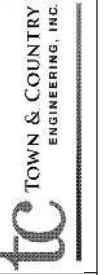
2021 STREET AND STORM SEWER IMPROVEMENTS
Renee Court, Pleasant Run, Lake Edge Road
Taylor-Farwell Storm Sewer
Village of McFarland, Wisconsin

PROJECT NO.:	MC 181
DRAWING FILE:	MC 181 DETAILS.DWG
DRAWN BY:	J.C.A.
CHECKED BY:	T.J.S.
DATE:	2-17-21
REVISIONS:	
SCALE:	N.T.S.
SHEET:	2

TEMPORARY BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
200	877.65	CHISELED 'X' ON BOC IN WEST QUADRANT OF RENEE CT & LEWIS LN



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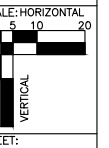


PLAN & PROFILE
RENEE COURT
Station 10+80 To Station 16+80

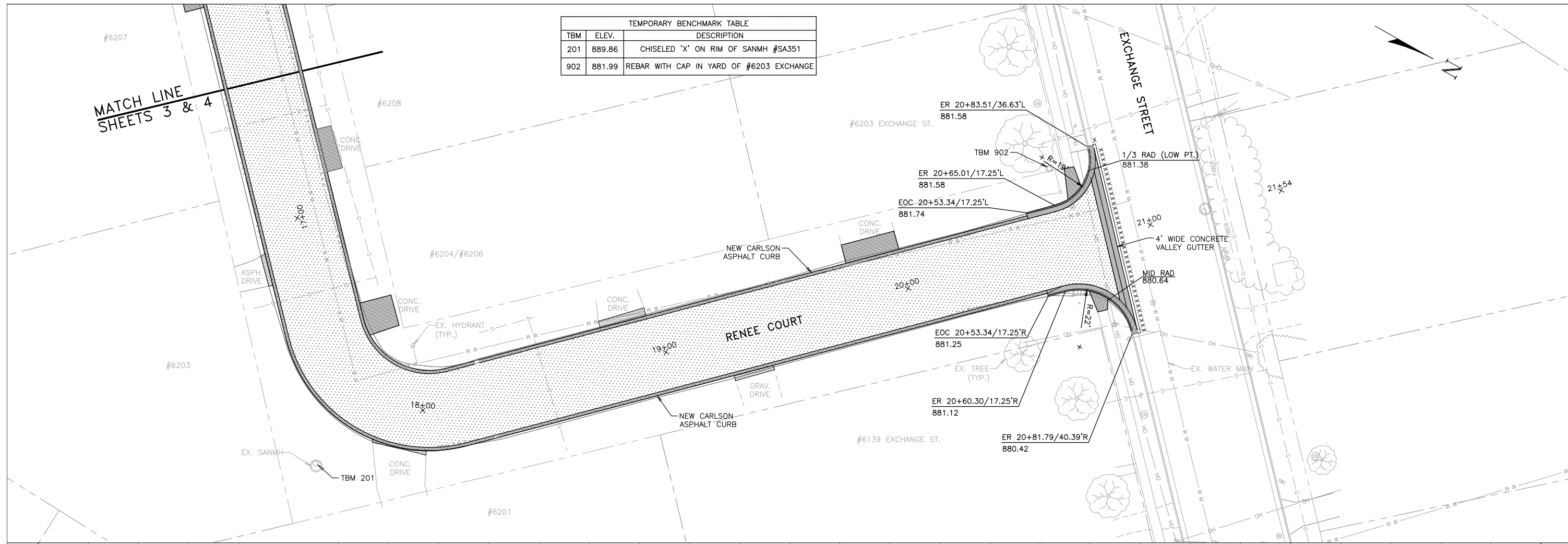
2021 STREET AND STORM SEWER IMPROVEMENTS
Renee Court, Pheasant Run, Lake Edge Road,
Taylor-Farwell Storm Sewer
Village of McFarland, Wisconsin

PROJECT NO.: MC 181
DRAWING FILE: MC 181 SHEETS.DWG
DRAWN BY: J.C.A.
CHECKED BY: T.J.S.

DATE: 2-17-21
REVISIONS:



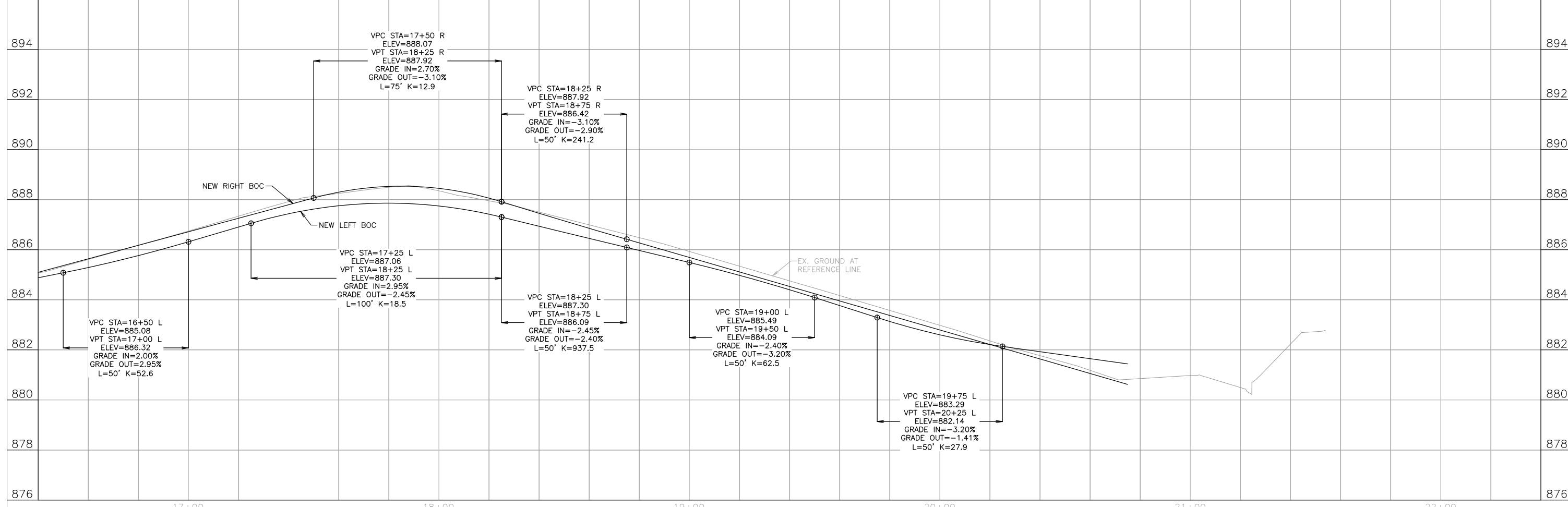
SHEET: 3



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 www.tceengineers.net

TC TOWN & COUNTRY ENGINEERING, INC.

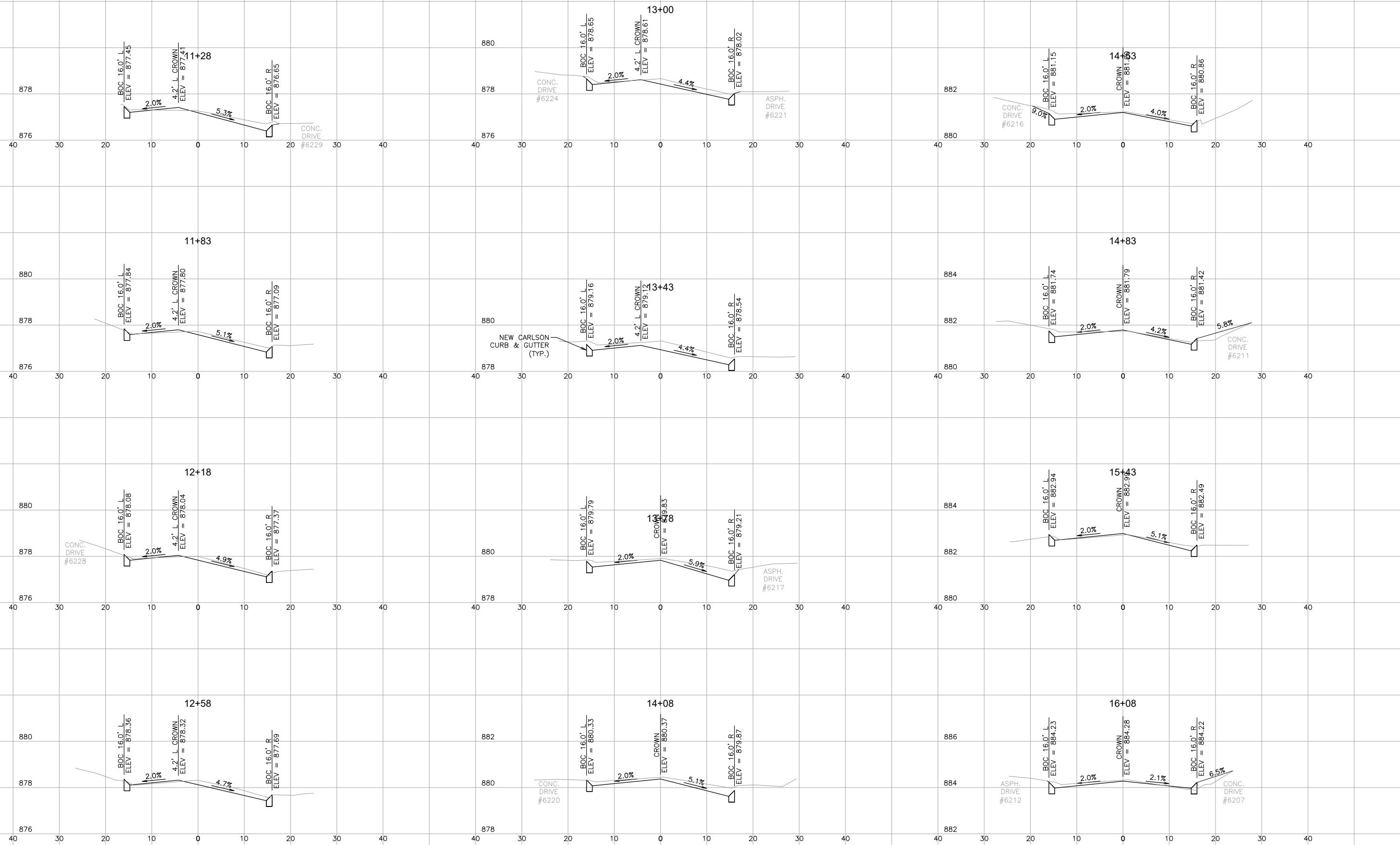
PLAN & PROFILE
 RENEÉ COURT
 Station 16+40 To Station 22+40



2021 STREET AND STORM SEWER IMPROVEMENTS
 Renee Court, Pheasant Run, Lake Edge Road,
 Taylor-Farwell Storm Sewer
 Village of McFarland, Wisconsin

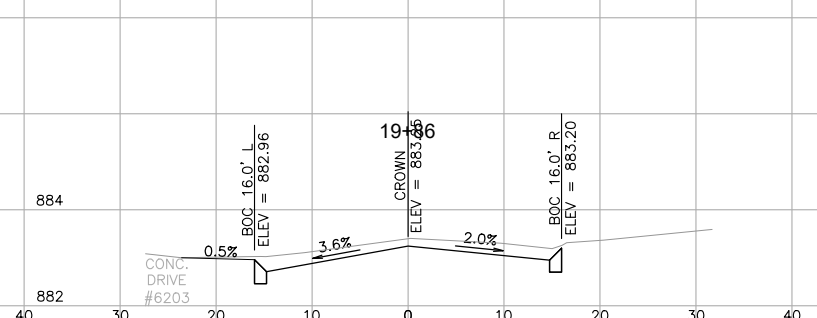
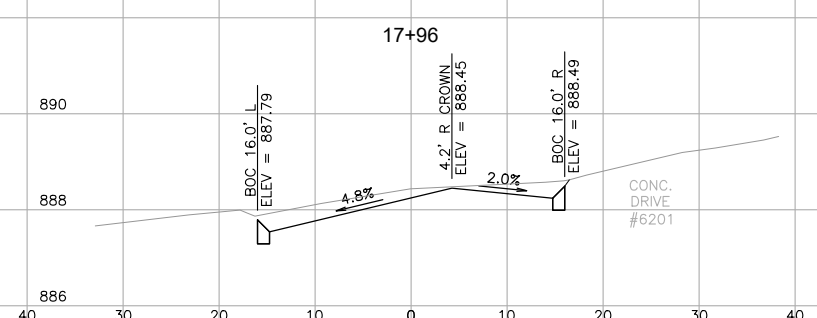
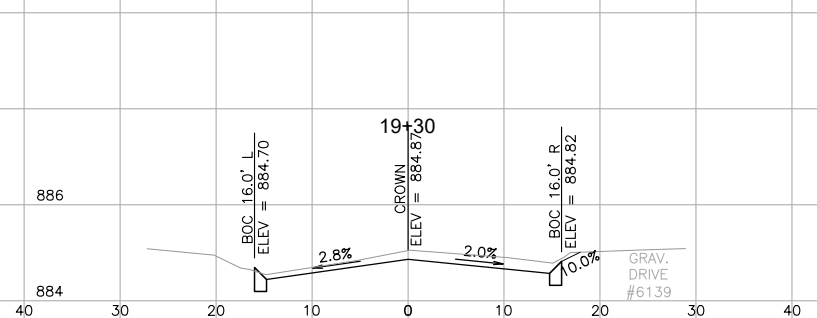
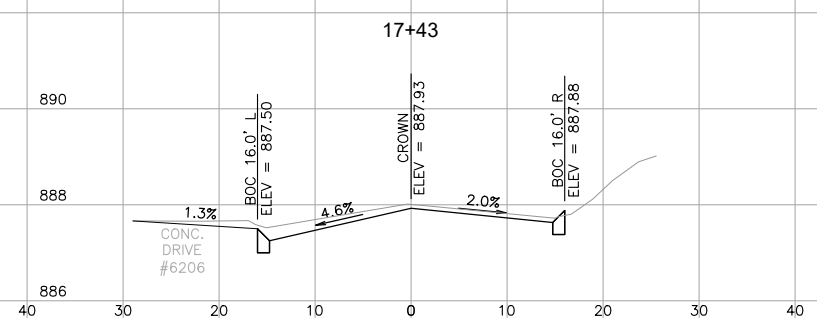
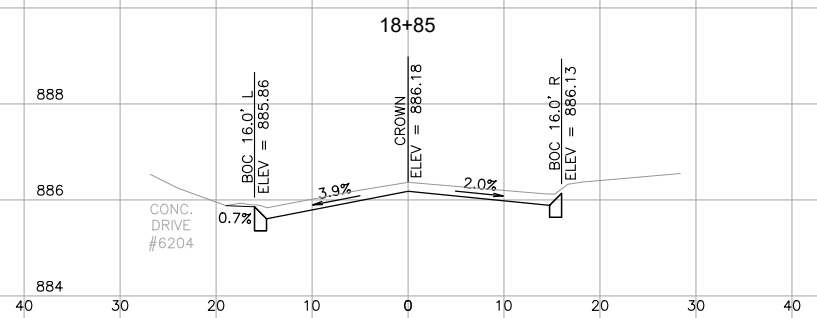
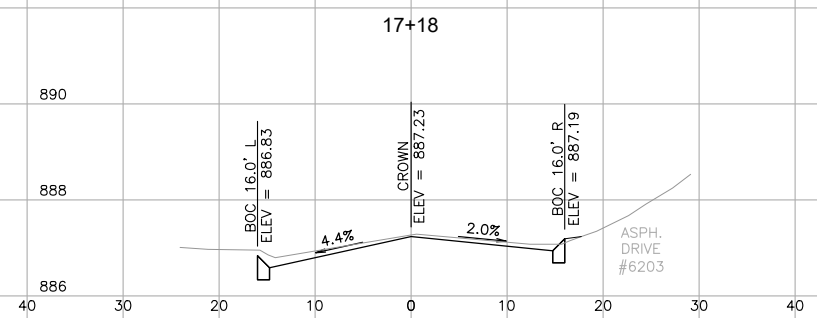
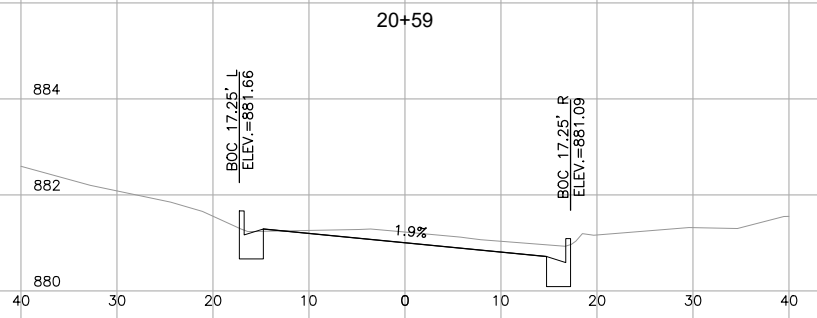
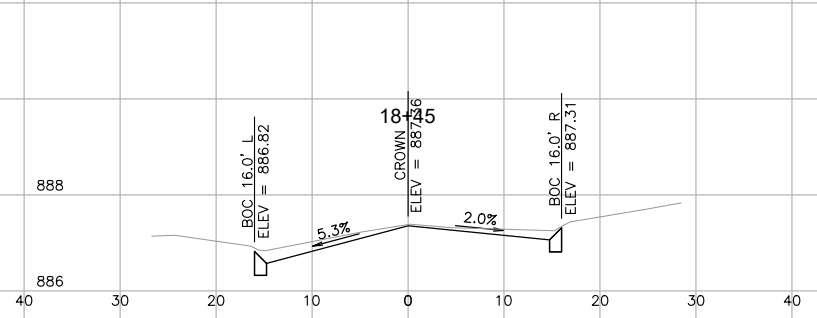
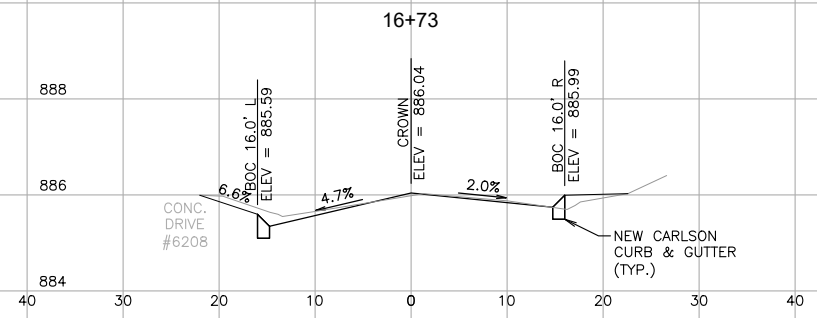
PROJECT NO.: MC 181
 DRAWING FILE: MC 181 SHEETS.DWG
 DRAWN BY: J.C.A.
 CHECKED BY: T.J.S.
 DATE: 2-17-21
 REVISIONS:
 SCALE: HORIZONTAL 1"=50'
 VERTICAL 1"=10'
 SHEET: 4

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL
 CURB HEIGHT.

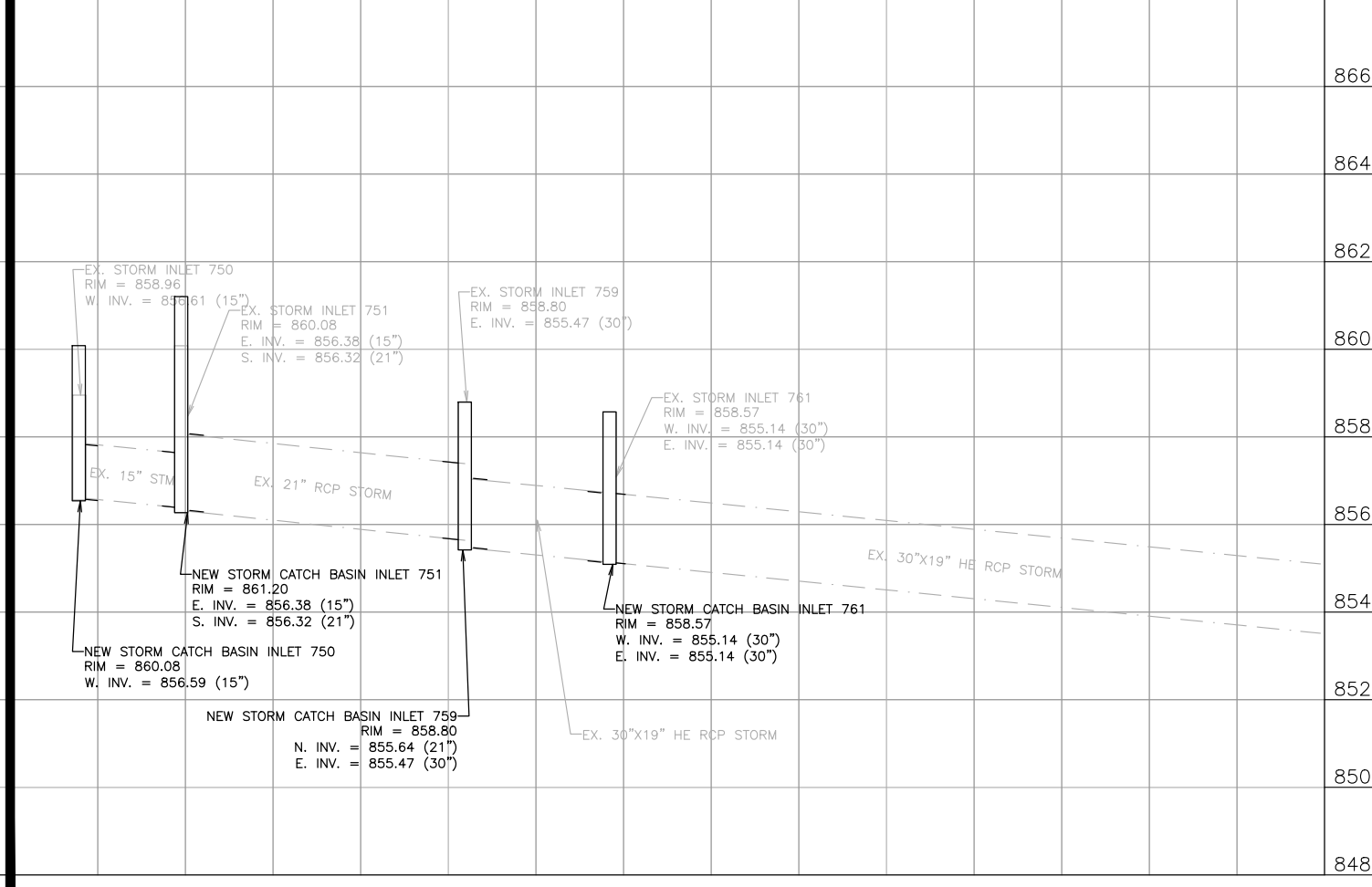
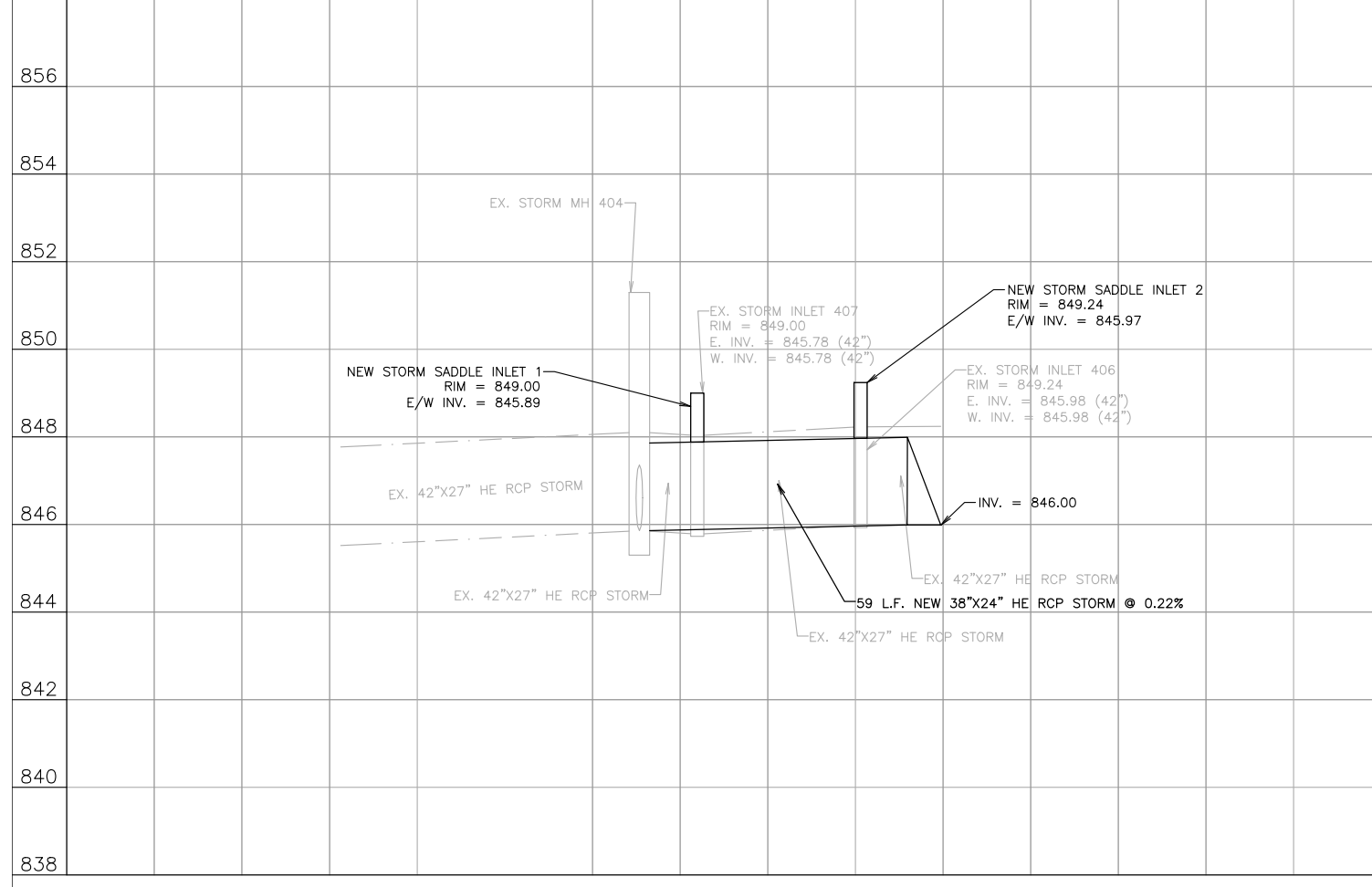
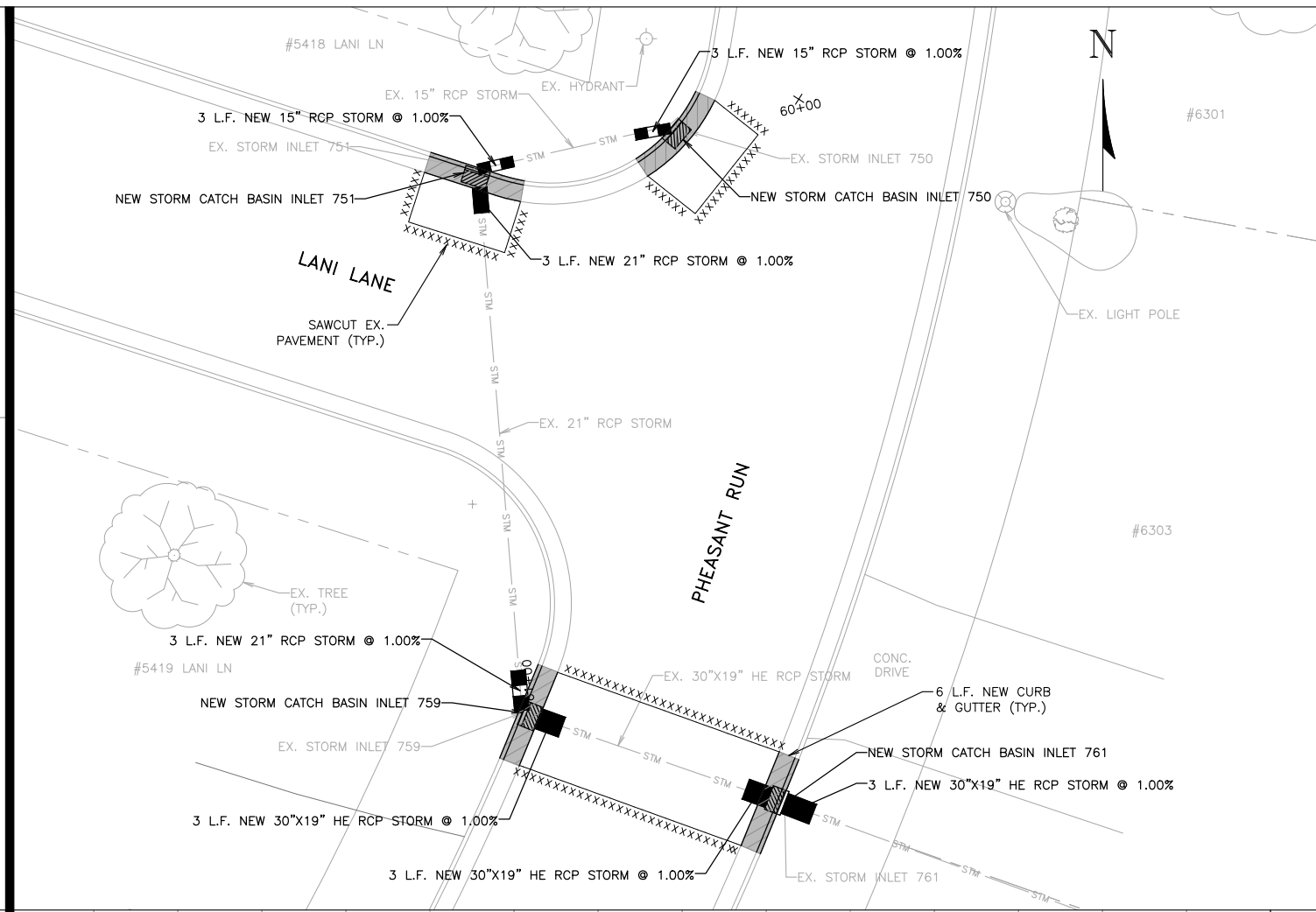
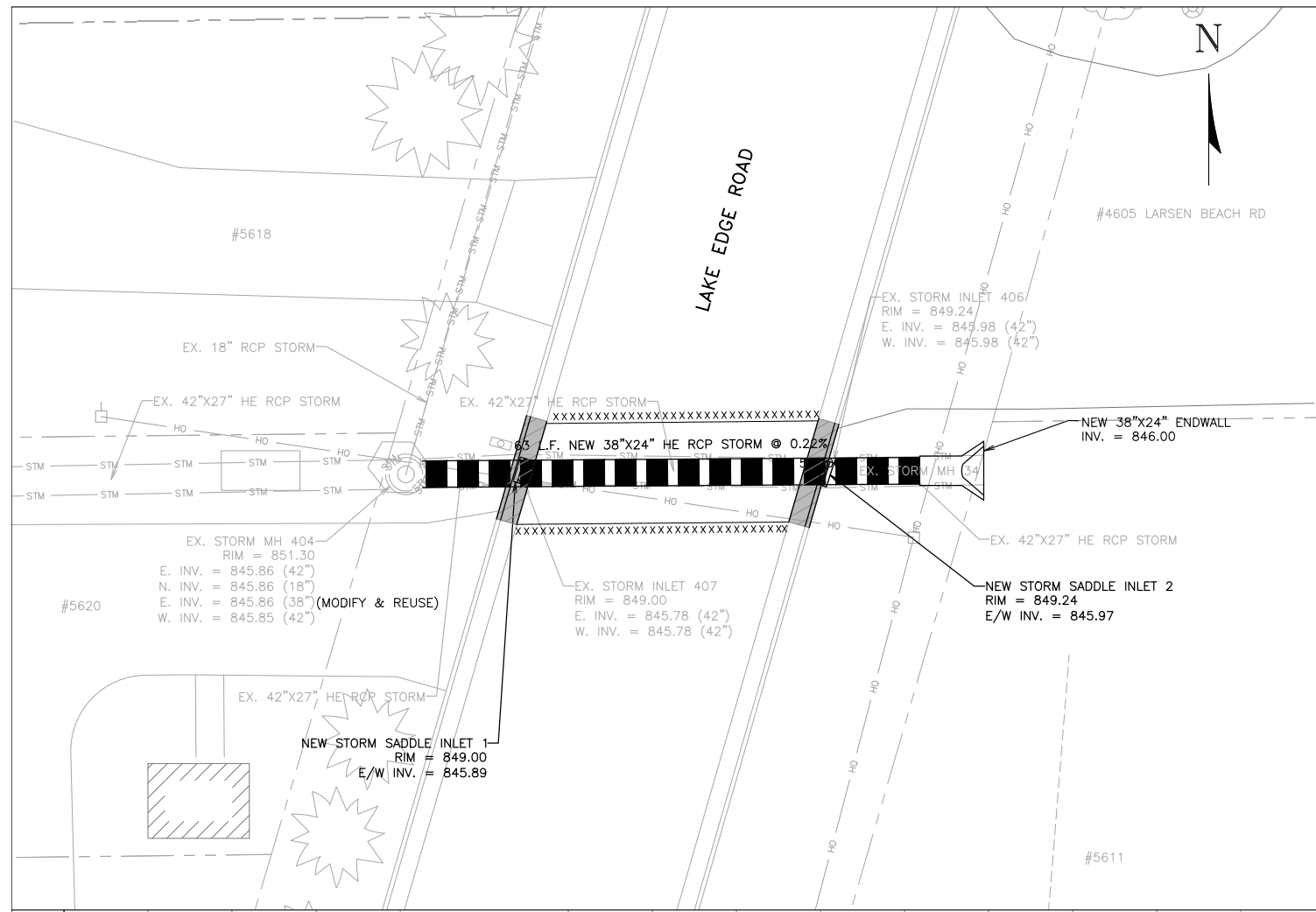


PROJECT NO.:	MC 181
DRAWING FILE:	MC 181 SHEETS.DWG
DRAWN BY:	J.C.A.
CHECKED BY:	T.J.S.
DATE:	2-17-21
REVISIONS:	
SCALE: HORIZONTAL	1" = 25'
SCALE: VERTICAL	1" = 5'
SHEET:	5

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL
 CURB HEIGHT.



PROJECT NO.:	MC 181
DRAWING FILE:	MC 181 SHEETS.DWG
DRAWN BY:	J.C.A.
CHECKED BY:	T.J.S.
DATE:	2-17-21
REVISIONS:	
SCALE: HORIZONTAL	1" = 20'
SCALE: VERTICAL	1" = 5'
SHEET:	6



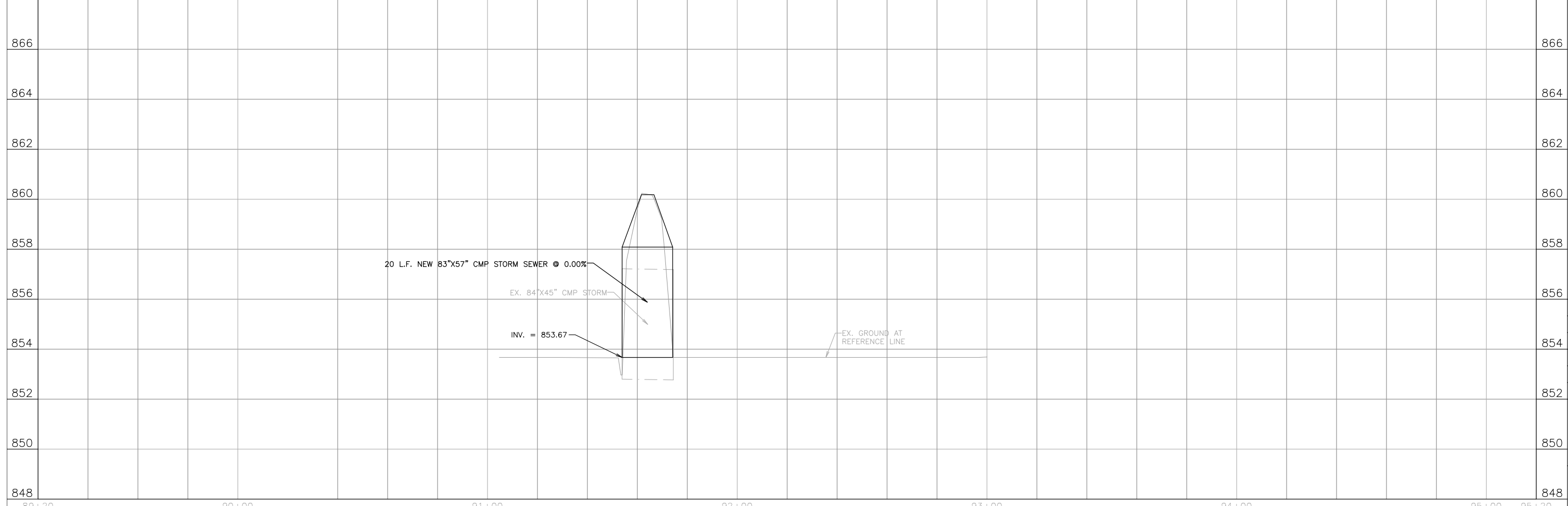
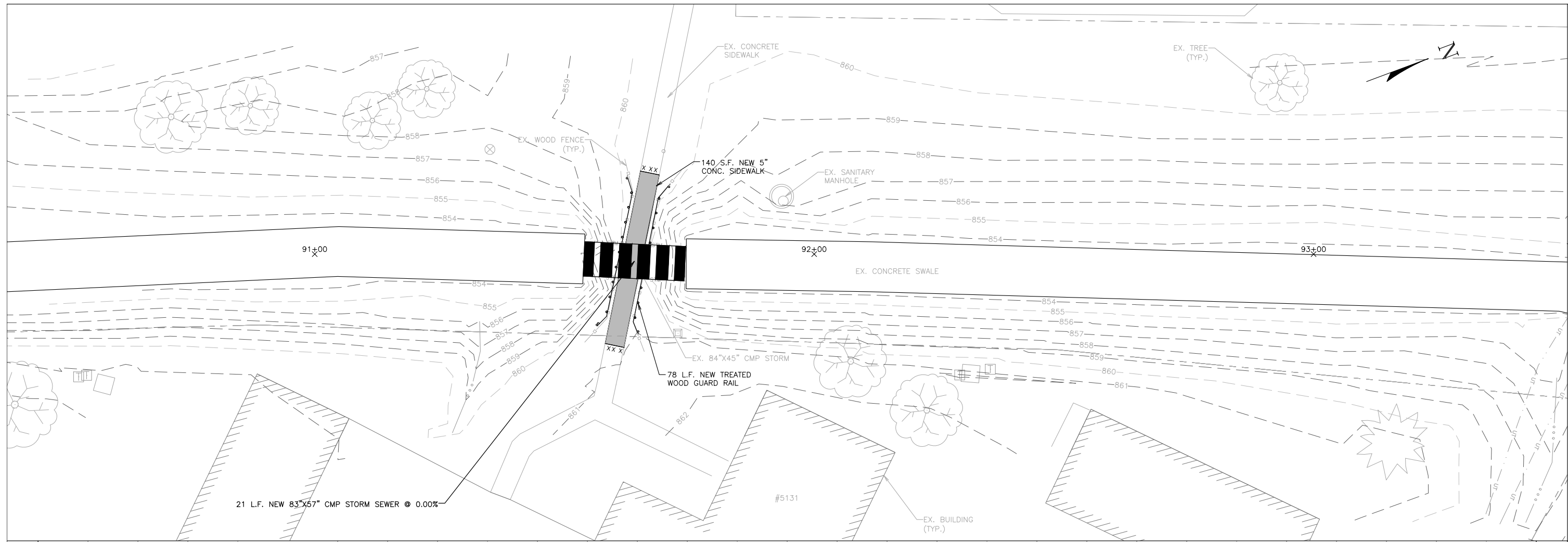
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TOWN & COUNTRY
ENGINEERING, INC.

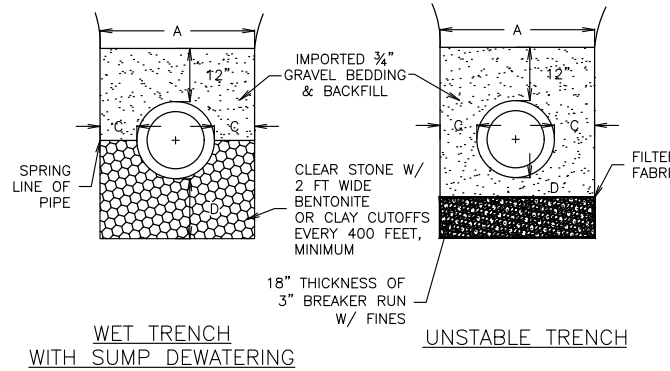
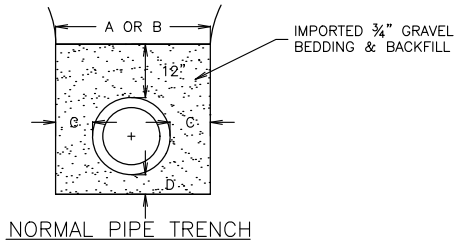
PLAN & PROFILE
PHEASANT RUN & LAKE EDGE ROAD

2021 STREET AND STORM SEWER IMPROVEMENTS
Renee Court, Pheasant Run, Lake Edge Road,
Taylor-Farwell Storm Sewer
Village of McFarland, Wisconsin

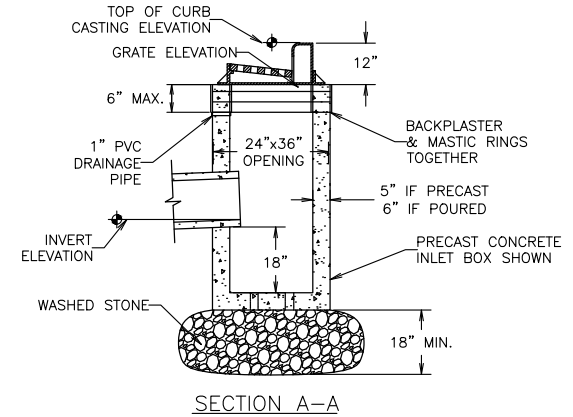
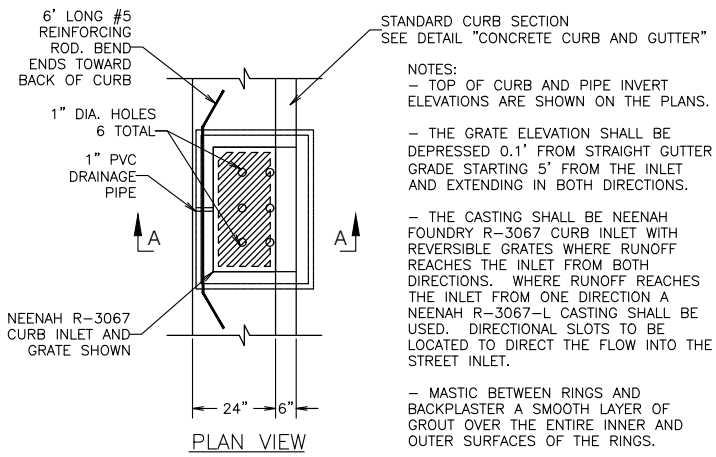
PROJECT NO.: MC 181
DRAWING FILE: MC 181 SHEETS.DWG
DRAWN BY: J.C.A.
CHECKED BY: T.J.S.
DATE: 2-17-21
REVISIONS:
SCALE: HORIZONTAL 0 2.5 5 10
VERTICAL 1 2
SHEET: 7



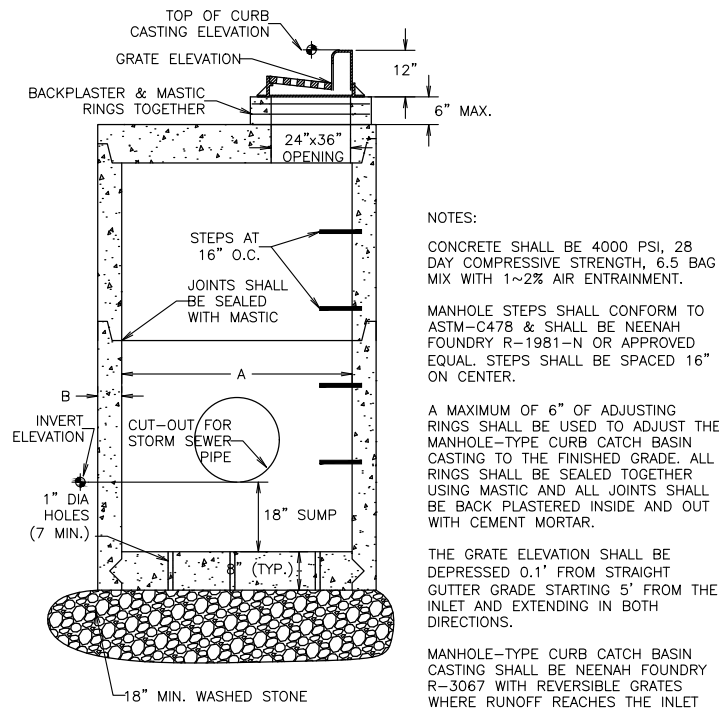
DIMENSIONS:
 A: OUTSIDE DIAMETER OF PIPE PLUS 24" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36". TRENCH SHIELDS NARROWER THAN 4 FEET INSIDE WIDTH WILL NOT BE REQUIRED UNLESS SPECIFICALLY REQUIRED IN THE PROJECT SPECIFICATIONS.
 B: FOR ROCK, OUTSIDE DIAMETER OF PIPE PLUS 18" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36".
 C: MINIMUM - 6"
 D: MINIMUM 4" BELOW BARREL AND 3" BELOW BELL.



DETAIL
TRENCH WIDTH AND BEDDING



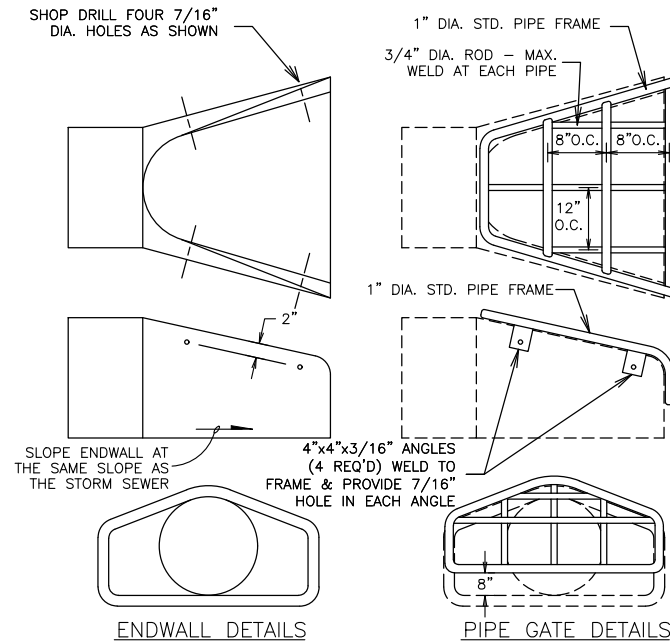
DETAIL
RECTANGULAR CATCH BASIN



STORM MANHOLE DIMENSIONS

MANHOLE SIZE	DIMENSION	
	A	B (MIN.)
48"	48"	5"
60"	60"	6"
72"	72"	7"
84"	84"	7"
96"	96"	9"

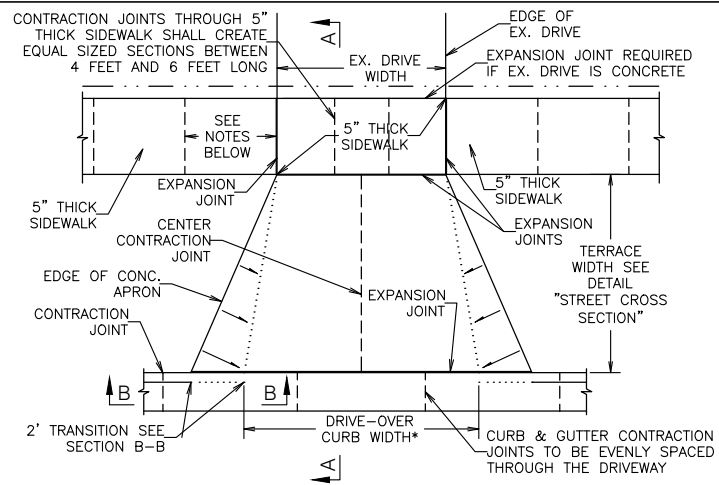
DETAIL
STORM SEWER MANHOLE CATCH BASIN



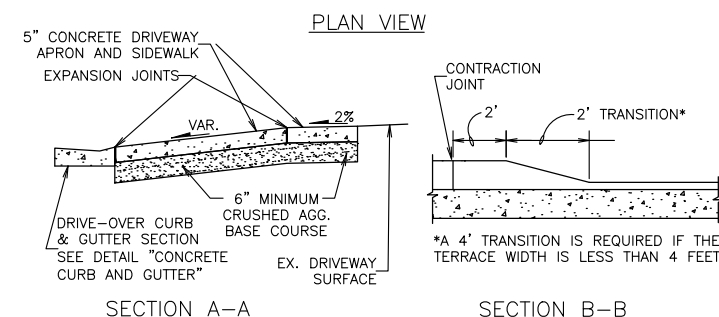
NOTES:
 - THE CONTRACTOR SHALL BOLT THE PIPE GATE TO THE CONCRETE ENDWALL WITH FOUR 3/8"x6" MACHINE BOLTS WITH NUTS ON INSIDE WALL.

PAINTING SPECIFICATIONS:
 - THE PIPE GATE SHALL RECEIVE THE FOLLOWING PREPARATION & PAINTING. THE FIRST COAT SHALL BE RUS-OLEUM X-60 RED BARE METAL PRIMER OR APPROVED EQUAL. THE SECOND COAT SHALL BE RUS-OLEUM 960 ZINC CHROMATE PRIMER OR APPROVED EQUAL. THE THIRD COAT SHALL BE RUS-OLEUM 1282 HIGH GLOSS METAL FINISH OR APPROVED EQUAL.

PREPARATION STEPS:
 1. BARE METAL SURFACES - TREAT WITH THE THREE-COAT PAINTING SYSTEM LISTED AFTER A THOROUGH SCRAPING, WIRE BRUSHING & CLEANING.
 2. EACH COAT OF PAINT SHALL BE APPLIED OVER THE ENTIRE GATE SURFACE.
 3. ALLOW 24-48 HOURS DRYING TIME AT 60° OR ABOVE BETWEEN COATS.



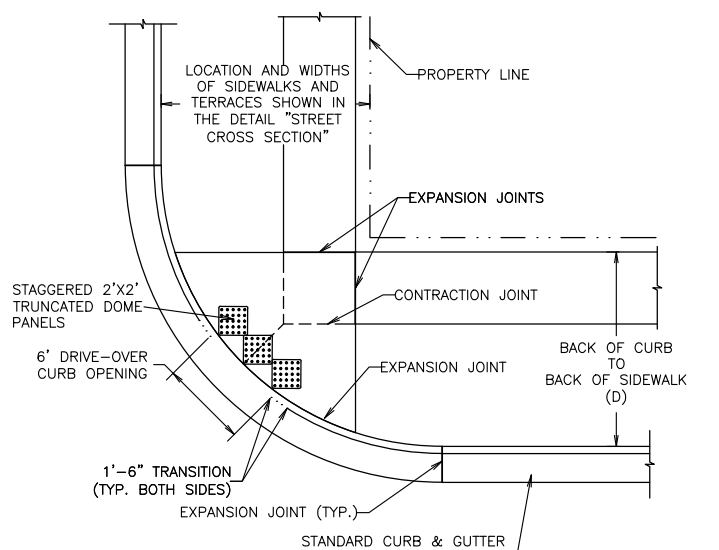
NOTES: - 5" THICK WALK WIDTH SHALL BE EQUAL TO THE EX. DRIVE WIDTH OR A MINIMUM OF 12' CENTERED ON THE EX. DRIVE. THE DRIVE-OVER CURB WIDTH SHALL BE EQUAL TO THE 5" THICK WALK WIDTH PLUS 2', CENTERED ON THE EX. DRIVE.
 - CONTRACTION JOINTS IN 5" THICK SIDEWALKS SHALL BE PLACED EVERY 5'. EXPANSION JOINTS SHALL BE PLACED AT INTERVALS NOT TO EXCEED 96 FEET.



DETAIL DRIVEWAY

TYPE 1 CURB RAMP

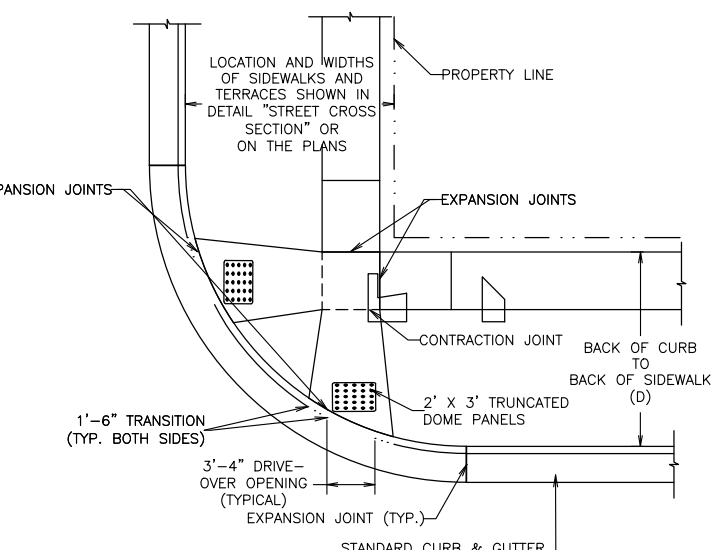
- FOR USE WHEN THE DISTANCE FROM THE BACK OF THE CURB TO THE BACK OF THE SIDEWALK (D) IS LESS THAN 12 FEET.



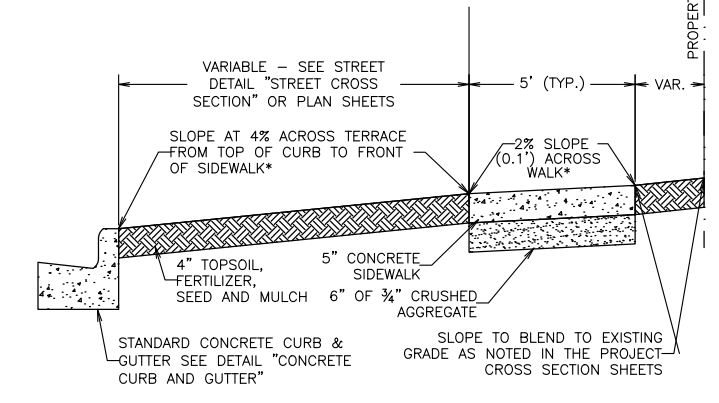
DETAIL CURB RAMP

TYPE 2 CURB RAMP

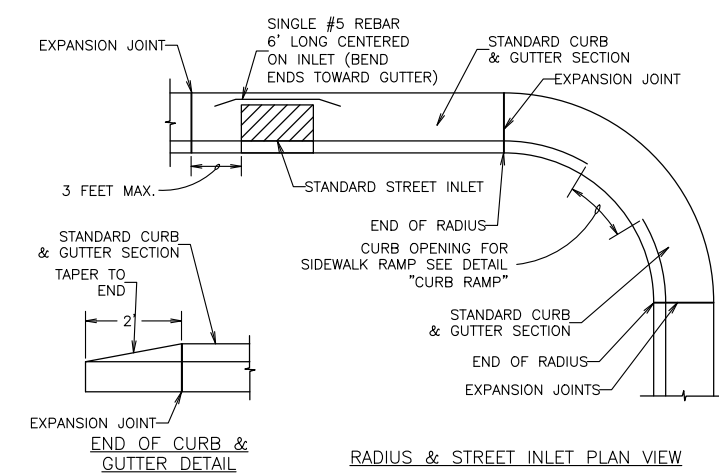
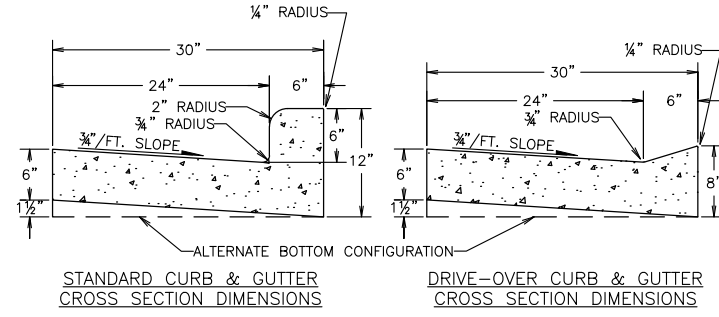
- FOR USE WHEN THE DISTANCE FROM THE BACK OF THE CURB TO THE BACK OF THE SIDEWALK (D) IS GREATER THAN OR EQUAL TO 12 FEET.



DETAIL CURB RAMP

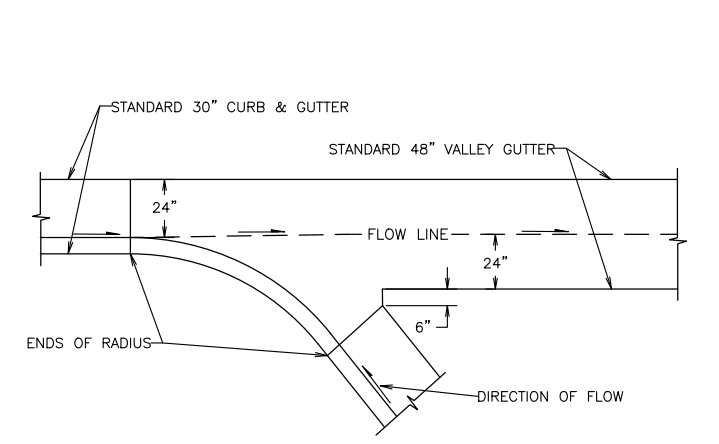


DETAIL SIDEWALK - TERRACE SECTION

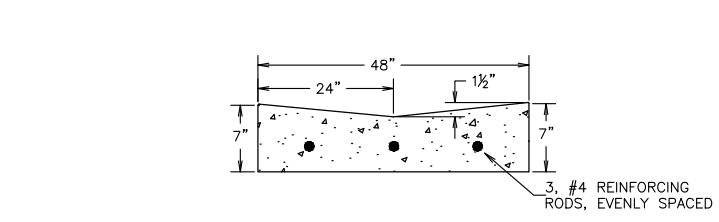


NOTES:
 1.) CONTRACTION JOINTS SHALL BE PLACED EVERY 6 TO 12 FEET AND AT LOCATIONS SHOWN IN THE CURB RAMP AND DRIVEWAY DETAILS.
 2.) EXPANSION JOINTS SHALL BE PLACED AT EVERY END OF RADIUS, 3 FEET ON ONE SIDE OF EACH STREET INLET AND AT INTERVALS NOT TO EXCEED 300 FEET.

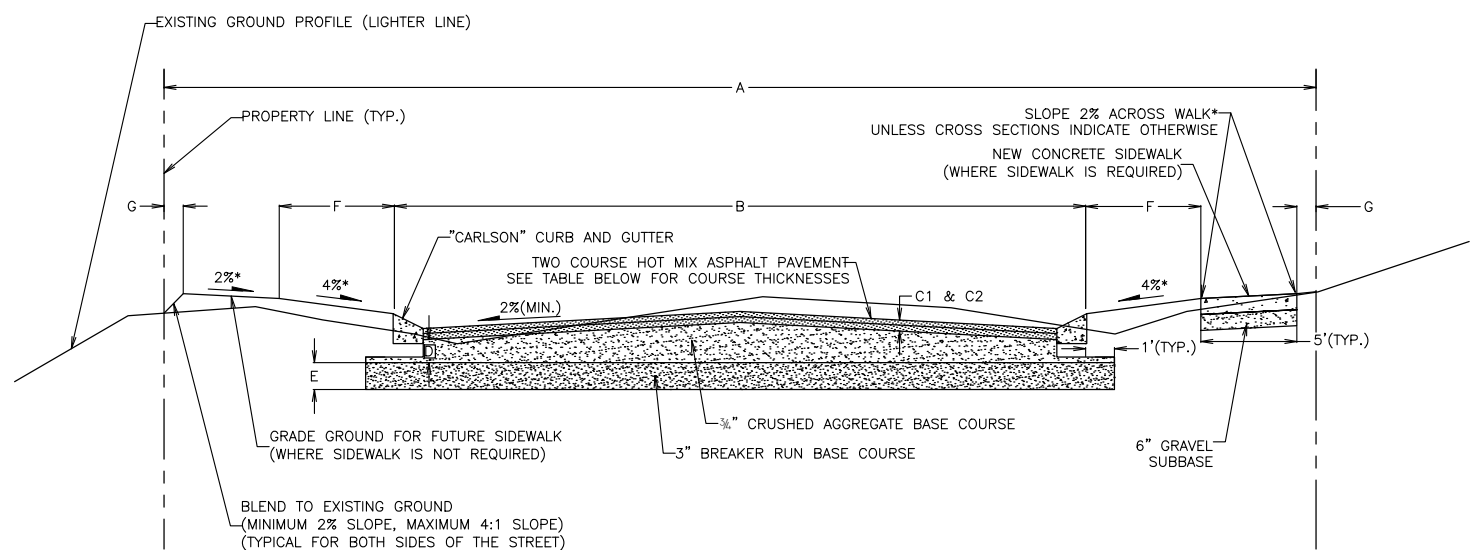
DETAIL CONCRETE CURB AND GUTTER



VALLEY GUTTER PLAN VIEW



DETAIL VALLEY GUTTER



STANDARD STREET DETAIL DIMENSIONS

	A	B	C1	C2	D	E	F	G
STREET NAME	RIGHT OF WAY WIDTH	CURB FACE TO CURB FACE WIDTH	LOWER COURSE THICKNESS	SURFACE COURSE THICKNESS	C.A.B.C. THICKNESS*	3" BREAKER RUN B.C. THICKNESS	TERRACE WIDTH	BACK OF WALK TO PROP. LINE
RENEE COURT			1 1/4"	1 1/2"				

*WHERE PLAN CROSS SECTIONS CONFLICT WITH THIS DETAIL, THE PLAN CROSS SECTION SHALL GOVERN.

NOTES:
 THE CROWN OF THE ROAD SHALL BE CREATED USING THE 3/4" CRUSHED AGGREGATE BASE COURSE. THE THICKNESS SHOWN IS THE MINIMUM THICKNESS REQUIRED AS MEASURED AT THE CONCRETE CURB & GUTTER SECTION.
 THE 3" BREAKER RUN BASE COURSE THICKNESS MAY NEED TO BE INCREASED DEPENDING UPON SUBGRADE CONDITIONS.

DETAIL STREET CROSS SECTION


VILLAGE OF
McFarland
SUMMARY SHEET

MEETING DATE: Tuesday, April 13, 2021

SECTION: Business

DEPARTMENT: Public Works

CONTACT: Jim Hessling, Public Works Director

AGENDA ITEM: Discussion and action to make a recommendation to the Village Board regarding 2021 Crackseal and Chipseal proposal for street maintenance.

PREVIOUS ACTION:

None.

ISSUE SUMMARY:

Annually the Village conducts cracksealing and chipseal work in order to maintain and extend the life of our streets. This type of work is common practice in municipalities.

Cracksealing is the practice of routing out or opening up a crack in the street and then filling it with a rubberized asphalt type of material to seal up the crack. This helps in keeping water out of the crack along with the asphalt and base material, extending the life of our streets..

Chip sealing is performed after the crack sealing process. Chip sealing consists of sweeping/cleaning off the street, laying down a tar type with a fine stone material applied directly behind the tar. The chips are then roller into the tar and in a few days the excess material is swept up and hauled away.

Enclosed in your packet is a proposal from Fahrner Asphalt Sealers for the 2021 work to be conducted on identified streets. Funds for this work have been allocated in the 2021 budget.

Fahrner Asphalt, the proposal provider, has been in business since 1979 and has worked in the village in the past and we have been very pleased with their work. . They are a reputable and well respected company.

FINANCIAL/BUDGET IMPACT:

The funds for this work will come out of account 100-53000-230-000 in which \$90,000 is allocated.

VILLAGE PLAN REFERENCE:

None.

ORDINANCE REFERENCE:

None.



BOARD, COMMISSION OR COMMITTEE RECOMMENDATION:

Recommendation to the Village Board approving the 2021 Chipseal and Cracksealing proposal provided by Fahrner Asphalt in the amount of \$78,275.

ATTACHMENTS:

1. 2021 Road Work Proposal
2. 2021 Road Matrix Bid Doc
3. Infrared Seamless Patching Spec
4. Joint & crack Sealing 2020
5. FA2 Washed chip Spec - Polymer

PROPOSAL / CONTRACT

Job. No. _____

Date: April 06, 2021

PLOVER, WI 54467
2800 Mecca Drive
Ph.: 715.341.2868
Fax: 715.341.1054

WAUNAKEE, WI 53597
316 Raemisch Road
Ph.: 608.849.6466
Fax: 608.849.6470

KAUKAUNA, WI 54130
860 Eastline Road
Ph.: 920.759.1008
Fax: 920.759.1019

EAU CLAIRE, WI 54703
6615 U.S. Hwy 12 W
Ph.: 715.874.6070
Fax: 715.874.6717



Pavement Maintenance Contractors

EEO/AA Employer

CORPORATE OFFICE: 1.800.332.3360

FREDERIC, WI 54837
3468 115th Street
Ph.: 715.653.2535
Fax: 715.653.2553

SAGINAW, MI 48601
2224 Veterans Memorial Pkwy
Ph.: 989.752.9200
Fax: 989.752.9205

DUBUQUE, IA 52002
7680 Commerce Park
Section C
Ph.: 563.556.6231
Fax: 563.588.1240

OAKDALE, MN 55128
7500 Hudson Blvd., Ste. 305
Ph.: 651.340.6212
Fax: 651.340.6221

Contact Name: Jim Hessling	Contract Price	\$78,275.00
PURCHASER: VILLAGE OF MCFARLAND	TELEPHONE:	(608) 838-7287
ADDRESS: 5115 TERMINAL DR MC FARLAND, WI 53558	DESCRIPTION OF PROPERTY:	2021 Street Maintenance 5155 Terminal Dr. McFarland, WI 53558

1. FAHRNER Asphalt Sealers, L.L.C. (CONTRACTOR) and PURCHASER agree that, CONTRACTOR shall furnish the labor and materials to complete certain construction in accordance with the following specifications:

2021 Infrared, Crackseal & Chipseal Proposal

Repair asphalt areas using Infrared Seamless Patching System.

Rout out cracks to a 3/4 x 3/4 ratio width versus depth. Blow out and clean cracks with compressed air and heat lance. Seal cracks with a rubberized asphalt crack sealant. This material exceeds Fed Spec ASTM D6690 Type II.

Thoroughly clean the existing surface with a self propelled broom. Apply 170 degree emulsified asphalt by full-width spray distributor. Apply cover aggregate and spread uniformly over surface with full-width self propelled chip spreader. Roll with pneumatic-tired roller.

- Emulsion - CRS2-P Polymer Modified
- Aggregate - FA2 Fractured Granite
- Sweeping - Included

**See Attached List of Roads and Specifications

This proposal may be withdrawn if not accepted and received by CONTRACTOR within 30 days of the date above and/or at any time before performance of the work hereunder upon CONTRACTOR'S determination that the PURCHASER is not creditworthy.

- 2. If proposal is accepted please sign, retain one copy and forward a copy to our office.
- 3. The undersigned ("PURCHASER") agrees to pay CONTRACTOR the total price of \$78,275.00 and/or the unit prices specified above for the labor and materials specified above which payment shall be due upon completion of each stage of work. PURCHASER acknowledges that the specifications, conditions and price quotes specified above are satisfactory and hereby accepted.

Acceptance of this Proposal includes acceptance of all the terms and conditions on back.

CONTRACTOR:
Fahrner Asphalt Sealers, LLC:608-280-1452
mike.jenquin@fahrnerasphalt.com

PURCHASER:
I have read and understand the terms and conditions on both sides of this contract.

Mike Jenquin

(PRINT OR TYPE NAME)

(PRINT OR TYPE NAME)

By: Mike Jenquin
(CONTRACTOR REPRESENTATIVE)

By: _____
(PURCHASER AUTHORIZED REPRESENTATIVE)

Date: April 06, 2021

Date of acceptance: _____

TERMS AND CONDITIONS

NOTICE OF LIEN RIGHTS

AS REQUIRED BY THE WISCONSIN CONSTRUCTION LIEN LAWS, CONTRACTOR HEREBY NOTIFIES OWNER THAT PERSONS OR COMPANIES FURNISHING LABOR OR MATERIALS FOR THE CONSTRUCTION ON OWNER'S LAND MAY HAVE LIEN RIGHTS ON OWNER'S LAND AND BUILDING IF NOT PAID. THOSE ENTITLED TO LIEN RIGHTS, IN ADDITION TO THE UNDERSIGNED CONTRACTOR, ARE THOSE WHO CONTRACT DIRECTLY WITH THE OWNER OR THOSE WHO GIVE THE OWNER NOTICE WITHIN SIXTY (60) DAYS AFTER THEY FIRST FURNISH LABOR OR MATERIALS FOR THE CONSTRUCTION. ACCORDINGLY, OWNER PROBABLY WILL RECEIVE NOTICES FROM THOSE WHO FURNISH LABOR OR MATERIALS FOR THE CONSTRUCTION, AND SHOULD GIVE A COPY OF EACH NOTICE RECEIVED TO THE MORTGAGE LENDER, IF ANY. CONTRACTOR AGREES TO COOPERATE WITH THE OWNER AND THE OWNER'S LENDER, IF ANY, TO SEE THAT ALL POTENTIAL LIEN CLAIMS ARE DULY PAID.

ACCEPTANCE OF WORK

All labor and material is conclusively accepted as satisfactory unless excepted to in writing within seven (7) days of performance.

EXTRA WORK

All alterations or deviations from any of the terms of this contract shall be in writing and executed by the parties hereto. Any extra cost involved therein will become an extra charge to be paid by PURCHASER over and above the contract price.

PURCHASER'S RESPONSIBILITIES

PURCHASER acknowledges and understands that it shall be responsible for obtaining any and all permits which may be required in connection with performance of this Proposal/Contract. Where applicable, PURCHASER shall also be responsible for backfilling areas that border along the newly paved surface with appropriate material to eliminate potential cracking and uneven surface at the edge of the paved surface and for installing, replacing, maintaining and repairing shoulders. PURCHASER assumes all liability for any damages done to underground utilities and/or structures unless CONTRACTOR has been notified, upon acceptance of this Proposal, as to the specific location and depth of any such buried utility/structures.

Unless exempt, in accepting this Proposal/Contract, PURCHASER acknowledges that it shall comply with the requirements of all applicable federal, state, and local employment laws, executive orders, codes and regulations (the "Requirements") effective where the work and/or services are being performed including, but not limited to, 41 CFR §§ 60-1.4(a)(1)-(8), 60-1.4(b), 60-1.4(c) or their successors effective where this Proposal/Contract is performed. To the extent required by law, all provisions of the Requirements are hereby incorporated into and made a part of this Agreement and any applicable agreements of CONTRACTOR. To the extent applicable, the Requirements include, but are not limited to, (1) prohibition of discrimination because of protected veteran status, disability, race, color, religion, sex, sexual orientation, gender identity, national origin and because of inquiry or discussion about or disclosure of compensation, (2) affirmative action to employ and advance in employment individuals without regard to race, sex, disability, or protected veteran status, (3) compliance with the Employee Notice clause contained in 29 C.F.R. part 471, Appendix A to Subpart A, or its successors, (4) compliance with the EEO-1 and VETS-4212 reports filing requirements in 41 C.F.R. §§ 60-1.7 and 41 C.F.R. §61-300.10, or their successors, (5) compliance with paycheck transparency obligations of 48 C.F.R. § 22.2005, including the contract clause found at 48 C.F.R. § 52.222-60, which is incorporated by reference as if fully set forth herein, (6) other affirmative action in employment, (7) required/certified payrolls, (8) social security acts, (9) unemployment compensation acts, (10) worker's compensation acts, (11) equal employment opportunity acts and (12) the required contract provisions for Federal-Aid Construction Contracts, Form FHWA-1273, if applicable.

When applicable, PURCHASER and CONTRACTOR shall abide by the requirements of 41 CFR 60-300.5(a). This regulation prohibits discrimination against qualified protected veterans, and requires affirmative action by covered prime contractors and subcontractors to employ and advance in employment qualified protected veterans. When applicable, PURCHASER and CONTRACTOR shall abide by the requirements of 41 CFR 60-741.5(a). This regulation prohibits discrimination against qualified individuals on the basis of disability, and requires affirmative action covered by prime contractors and subcontractors to employ and advance in employment qualified individuals with disabilities.

INCLEMENT WEATHER

Inclement weather may alter the completion of the work to be furnished hereunder. Furthermore, special consideration should be given if work is to be performed before May 1 or after October 15 in light of less than desirable weather conditions which could potentially impair the quality of the work performed hereunder.

WARRANTY

All material is guaranteed to be as specified and all work is to be completed in a workmanlike manner according to standard practices. All labor and materials will be guaranteed against defect for one (1) year from date of installation. Due to Wisconsin winters and expansion and contraction of the ground, some cracking of the pavement may be experienced. There are no express or implied warranties of merchantability, quality, quantity or of fitness for any particular purpose, which extend beyond those specifically set out in this document.

All warranties are void if payment is not made as stipulated.

DELINQUENCY CHARGE

Payment is due and payable upon completion of each stage of the work. If PURCHASER defaults on the payment required, PURCHASER will be liable for all costs of collection, including reasonable attorney's fees, and a delinquency charge on the balance at the maximum rates allowed by law. If PURCHASER is an organization as defined by Wis. Statutes, Section 421.301(28), the Delinquency Charge rate shall be 1.5% per month (18% APR) plus all costs of collection, including reasonable attorney's fees. CONTRACTOR retains title to all merchandise covered by this Agreement until full payment is received according to the above terms of sale. PURCHASER consents in any action or legal proceeding relating to this Contract commenced by the CONTRACTOR to the personal jurisdiction of any court that is either a court of record in the State of Wisconsin or a court of the United States located in the State of Wisconsin. It is hereby agreed that no legal action with respect to this contract may be brought by either party later than one (1) year after the cause of action accrues and that the party asserting such a legal action shall be barred from any remedy thereto.

INDIVIDUAL LIABILITY

The undersigned PURCHASER agrees to be individually liable for all terms of the Agreement, regardless of whether he or she signs individually or as an agent for the owner of the property upon which the work is being performed or for any other individual, partnership or corporation.

PRODUCT INFORMATION AND MAINTENANCE

Since the asphalt in blacktop needs time to harden and cure, usually 6-12 months, your asphalt pavement will remain soft and pliable during warm weather. During this time, don't park in the same spot every time and do not turn your steering wheel back and forth when your car is not moving. It is not unusual to experience some cracking over the winter due to the contraction and expansion of the ground, especially over culverts, pipes, electric wires, etc. Avoid gasoline and petroleum product spills as they will destroy your pavement. If spills do occur, immediately flush with lots of soapy water. If you decide to seal coat your pavement, wait until the summer following installation.

BINDING EFFECT

This Agreement shall be binding upon the parties hereto, their heirs, personal representatives, successors and assigns.

ENTIRE AGREEMENT

The entire contract is embodied in this writing. This writing constitutes the final expression of the party's agreement, and is a complete and exclusive statement of that agreement. In the event that any term of this contract is unenforceable, the remaining terms of the contract shall still be in full force and effect.

Village of McFarland								
Road Maintenance 2021 - Specifications Attached								
Roadways:								
Road	Start		Stop		Crack Fill	Infrared	Chip Seal	Amount
Exchange Street	Farwell St.		Long St.		x			\$ 1,555.00
Main Street	Long St. / Anthony St.		Broadhead St.		x			\$ 660.00
Main Street	Church St. Intersection		Church St. Intersection		x			\$ 350.00
Broadhead Street	Main St.		Holscher Rd.		x			\$ 4,280.00
Holscher Road	Sigglekow Rd.		Broadhead St.		x			\$ 2,825.00
Alben Avenue	Scott St.		Broadhead St.		x	x	x	\$ 6,705.00
Alben Avenue	Scott St.		Scott St.		x		x	\$ 1,210.00
Farwell Street	Hwy 51		Exchange St.		x			\$ 1,295.00
Sparten Drive	Farwell St.		End		x		x	\$ 5,980.00
Lani Lane	Highland Dr. Intersection		Highland Dr. Intersection		x	x		\$ 700.00
Leanne Lane	Osborn Dr.		Broadhead St.				x	\$ 11,245.00
Osborn Drive	Anthony St.		5707 Osborn Dr.		x		x	\$ 12,570.00
Running Deer Trail	Osborn Dr.		Broadhead St.		x		x	\$ 12,020.00
Glenway Street	Osborn Dr.		Broadhead St.		x		x	\$ 14,080.00
Farwell Street	West of Exchange St.		West of Exchange St.			x		\$ 400.00
Farwell Street	West of Merrill St.		West of Merrill St.			x		\$ 800.00
Highland Drive	End of Highland Dr.		End of Highland Dr.			x		\$ 1,600.00
TOTALS								\$ 78,275.00

SPECIFICATIONS

INFRARED SEAMLESS PATCHING

Scope of Work: This work shall consist of furnishing all materials, equipment and labor necessary for the application of Infrared Seamless Patching.

Description: Infrared Thermal Bond Seamless Bituminous Pavement Patching is a method of blending new bituminous concrete material with infrared heated existing surface material to form a joint free integral mix patch.

Materials:

- A. New bituminous concrete material for patching shall be equal to Wisconsin Department of Transportation specifications gradation 3 or 4 Asphalt cement grade 120 – 150, is desirable.
- B. A minimum of 20% of new virgin material shall be added to all patched areas. Additional material shall be added as needed and as directed by the engineer.

Equipment:

- A. Pavement Restoration Vehicle (PRV) shall be a truck mounted, self contained pavement maintenance heating system equipped with a fuel system and a heated chamber capable of maintaining the fresh virgin bituminous concrete materials at a temperature of 275 degrees or higher. Any material with a laying temperature less than 265 degrees shall be discarded. Any material not used within 48 hours shall be discarded.
- B. The adjustable height infrared heating unit will be truck mounted to the PRV. The heating unit shall be equipped with a heating chamber or chambers capable of heating the existing bituminous pavement to a workable condition without oxidation or burning. There shall be no flame in direct contact with the existing bituminous surface.
- C. Compaction shall be achieved with a self-propelled vibratory roller of sufficient size to provide complete compaction to the full heated depth of the patched area. The new surface shall match the elevation of the adjacent pavement. Compaction methods shall be subject to the approval of the engineer.

Construction Method:

- A.** The areas to be patched shall be marked on the pavement and measured by the engineer and the contractor. The contractor shall determine the starting point of the work and the sequence to the heater application so as to accomplish all the work as specified.
- B.** The infrared heating unit shall be lowered to within six (6") inches of the existing pavement. The heated area must extend at least six (6") inches outside the area to repair.
- C.** Apply heat to the area continuously until the surface is heated to a depth of approximately 1 1/2 inches. The depth of heat penetration thru an existing bituminous overlay or surface course shall be the thickness of the surface course or approximately 1 1/2 inches, whichever is less. When the surface can be worked with a rake, proper heat penetration has been achieved.
- D.** Etch an outline of the perimeter of the repair area with the back of a rake at least three (3") inches beyond the edges of the repair area. Scarify the existing bituminous surface with the repair area to the full heated depth.
- E.** Remove enough existing bituminous material (as required by adjacent grades) to allow for the addition of 20% new virgin bituminous mix to achieve a blend of 20% new / 80% existing heated material within the area of the patch
- F.** If the patch area is already low, less existing material need be removed prior to the addition of the new virgin mix. Remove only the old oxidized surface in low areas.
- G.** Reshape patched area by hand with rake and lute to match grade of existing adjacent pavement.
- H.** Compact new paving with the specified roller to the full depth of the heated patch. Compacted surface shall be smooth, in texture and shall have positive drainage, matching the slope of the existing adjacent pavement.

- I. Clean-up site after paving. Remove all debris resulting from patching operation to the satisfaction of the engineer.

Insurance: No contractor shall commence work under this contract until he has obtained all insurance required under this paragraph.

Worker's Compensation and Employer's Liability

- A. \$100,000 each accident
- B. \$500,000 Disease-Policy limit
- C. \$100,000 Disease-each employee

General Liability

- A. \$2,000,000 General Aggregate
- B. \$2,000,000 Products
- C. \$1,000,000 Personal and Advertising
- D. \$1,000,000 Each Occurrence

Automobile Liability

- A. \$1,000,000 Combined Single Limit

Excess Liability

- A. \$4,000,000 Umbrella Form

Method of Payment: Measurement for payment shall be on a per heat basis and shall include all equipment, labor, materials and necessary incidentals for completion of the paving project.

Word/carol/specs/Infrared Seamless Patching

Specifications

Joint and Crack Sealant

Scope of Work

This work shall consist of furnishing all materials, equipment and labor necessary for the application of the Hot Pour Rubberized Joint Sealant as directed.

Description

The work shall consist of routing, cleaning, and sealing of random cracks and existing transverse and longitudinal joints.

Material

A. The material used must be a premium quality Rubber Asphalt joint Sealer. The sealer must **exceed the minimum ASTM D 6690 specification** for Hot Pour Rubber Asphalt Joint Sealants and contain a minimum of 38% rubber content.

B. The Sealant shall be in manufacturer's original sealed containers. Each container shall have the manufacturer's name, batch number, and manufacturer's recommendation for melting and application.

Equipment

A. A minimum of two dustless routers will be required, the routers must be a minimum of 24 H.P. using star wheel carbide tipped router blades attached to a main cutting head. It must have in-line wheels and cutting head capable of following random cracks. It must have an automatic depth control to insure consistent and accurate routing depths.

B. Two (2) air compressors will be required. They must be of sufficient size to maintain air pressure of 120 PSI and provide moisture and oil free compressed air. One (1) compressor shall be used with an air wand to blow out the crack and clean off the road. The second shall be used with the heat lance. Note that one compressor is not of sufficient size to run both air wand and heat lance at the same time.

C. The kettle used for heating the sealant must be an oil jacketed double boiler type melting unit, which is, equipped with both agitation and recirculation systems. It must have separate temperature thermometers for both the oil bath and melting vat to insure proper temperature for the sealant. It must be equipped with a pump to pressure fill cracks with a wand applicator.

Construction Method

All cracks and joints must be routed to a minimum of a $\frac{3}{4}$ " x $\frac{3}{4}$ " width versus depth. Cracks shall be blown out with 120 PSI compressed air. Immediately following routing, the contractor shall vacuum sweep up any debris on the roadway. Using air compressors to consolidate the debris is not allowed.

Using the second compressor the cracks shall be blown out using a heat lance. All cracks shall be pressure filled by a wand applicator from the bottom up. They shall be slightly over-filled and squeegeed to create an overband 1" wide on each side of the routed reservoir. The cracks will then be protected with single ply toilet paper or detach to prevent any material from tracking.

Debris Removal

Debris from the routing, sawing, crack preparation, and crack filling work shall be removed from the pavement surface by brooming, blowing with compressed air, or other methods satisfactory to the city representative. The work area shall be left in a broom clean condition at the end of the day.

Traffic Control

This project shall be kept open to traffic at all times. The contractor shall conduct his operations in a manner that will cause the least interference to traffic movements. The minimum number of vehicles of the contractor, his suppliers and his employees necessary for the prosecution of the work shall be permitted to park at the various work sites for the minimum time necessary for the performance of the work.

During nighttime hours, no equipment shall be parked or stored with twelve (12) feet of the near edge of the shoulder of the traveled roadway unless approved by the local municipality. Equipment not being used during the actual performance of the work shall not be parked or stored within the right of way unless otherwise approved by the local municipality.

Insurance

No contractor shall commence work under this contract until he has obtained all insurance required under this paragraph.

Worker's Compensation and Employers' Liability

- A. \$100,000 each accident
- B. \$500,000 Disease-Policy limit
- C. \$100,000 Disease-each employee

General Liability

- A. \$2,000,000 General aggregate
- B. \$2,000,000 Products

- C. \$1,000,000 Personal and Advertising
- D. \$1,000,000 Each Occurrence

Automobile Liability (Any Auto)

- A. \$1,000,000 Combined Single Limit

Excess Liability

- A. \$4,000,000 Umbrella Form

Method of Payment

Payment for crack sealing will be made by per pound. The price shall be full compensation for furnishing all materials. For all preparation, delivering and placing of these materials, and for all labor, equipment, tools and incidentals necessary to complete the work per specifications.

Qualification of Contractor

Contractor shall have been routing, cleaning, and sealing cracks per specification for a minimum of five (5) years. Contractor must be bonded and insured per owner's specifications.

Performance Qualifications

Contractor must have a minimum of five years experience in township cracksealing. Work must be within 25 miles for inspection and performance qualification.

Pre-Qualifications

All bidders must submit a list of five (5) previously completed crack sealing projects. All bidders must submit a list of equipment and operators to be used on project. All bidders must submit technical data stating why their material exceeds Federal Specification ASTM D3405.

Reference Projects Must Meet the Following Criteria

- A. Each project must have been in place for a minimum of three (3) winters.
- B. All cracks on sample project must have been routed prior to sealing, with polymeric sealant.
- C. Only projects completed on asphalt pavement involving public roads or streets and amounting to at least one-half (1/2) mile each will qualify. Parking lots will not qualify.
- D. Reference projects must be within 25 miles radius of the Township.

SPECIFICATION

BITUMINOUS SEAL COATING WITH FA2 WASHED CHIP COVER AGGREGATE

Scope of Work: This work shall consist of furnishing all materials, equipment, and labor necessary for the application of Bituminous Seal Coating as directed.

Description: The work shall consist of bituminous seal coating with stone cover aggregate. The work shall be done in accordance with Section 475, State of Wisconsin Department of Transportation “2017 Standard Specifications for Highway and Structure Construction” with the special provisions for aggregate below.

Materials: Materials furnished and used in the work shall conform to the requirements of Section 475. Aggregate must be a washed fractured granite stone classified as a Class A aggregate. Aggregate to be medium gray in color. Limestone, pea stone and boiler slag are not permitted.

A. Asphaltic Material Seal Coat.

Asphaltic material to be CRS-2P at the following:

Rate of application: 0.30 +/- .03 gallons/SY

Supplement 455.2.4.3 (1) of the standard specifications with the following:
Furnish material conforming, before dilution, to the following:

- I. Polymer Modified cationic emulsified asphalt..... AASHTO M 316

B. Cover Aggregate: The cover aggregate shall meet the requirements of The sieve specifications, and conform to the following sieve size.

Sieve Size	Percent Passing
½ inch	100 %
3/8 inch	100 %
¼ inch	100 %
No. 4	0- 100 %
No. 8	0 – 45 %
No. 16	0 – 15 %
No. 50	0 – 7 %
No. 100	0 – 4 %
No. 200	0 – 2 %

The aggregate shall be applied at the rate of 20 +/- 3 pounds per square yard. The Contractor shall take all precautions to minimize contamination of the aggregate.

Equipment: The contractor shall have available and maintain in good working order the equipment and tools necessary to perform the work. The requirements for the equipment for heating the asphaltic material and for the distributors shall be as set forth in subsection 475.3.4. The equipment to be used shall include dragging equipment and aggregate spreading equipment that can be adjusted to spread accurately the quantities specified per square yard, and a minimum of two self-propelled, pneumatic-tire rollers meeting the requirements of Subsection 475.3.5.

Construction Methods:

- A.** Surface Preparations: Immediately prior to applying the asphaltic materials, the Contractor shall thoroughly clean the existing surface of all loose material, silt spots, vegetation, and other objectionable material. Dust and other loose material in depressions or other places not reached by mechanical sweepers shall be swept with hand brooms or by blowers or flushers. Particular care shall be taken to thoroughly clean the outer edges of the area to be sealed. All costs associated with preparing existing surfaces as described above shall be included in the bid price for Construct Chip Seal Surface.
- B.** Applying Asphaltic Materials: A pressure distributor shall be used for applying the asphalt material. It shall have a ground speed control device interconnected with the asphalt emulsion pump such that specified application rate will be supplied at any speed. The pressure distributor shall be capable of maintaining the asphalt emulsion at the specified temperature. The spray bar nozzles shall produce a uniform fan spray, and the shutoff shall be instantaneous, with no dripping. Each pressure distributor shall be capable of maintaining the specified application rate. Means shall be provided for accurately indicating the temperature of the asphalt material at all times. The thermometer well shall not be in contact with a heating tube. Application will be with full width equipment capable of applying 24' without a center seam. A hose and spray nozzle attachment shall be provided for applying asphalt material to patches and areas inaccessible to the spray bar. The distributor shall be provided with heaters that can be used to bring the asphalt material to spray application temperature.
- C.** Applying Sealcoat Aggregate: After application of the asphaltic material and when the desired stage of tackiness is attained, aggregate for seal coat cover shall be spread uniformly over the treated surface by approved self-propelled mechanical full width spreader capable of applying aggregate 24' without a seam.
- D.** Rolling of Aggregate: Roll surface immediately after spreading the aggregate. Rolling shall start at the edges and continue to the center, lapping one-half the roller width on each successive trip. Rolling shall be accomplished with two pneumatic-tire rollers. The speed and reversing of direction of rollers shall be regulated as to avoid displacement or loosening of the cover material or damage to the asphaltic material. Rolling shall be continued until the aggregate for the seal coat cover is thoroughly embedded and the surface is smooth and uniform in texture.
- E.** Loose aggregate to be swept 24 to 48 hours after application.

Traffic Control: The contractor shall proceed in such manner as to interfere with traffic as little as possible. The Contractor shall provide flagmen to direct traffic on streets where travel will be restricted.

Insurance: No Contractor shall commence work under this contract until he has obtained all insurance required under this paragraph.

Worker's Compensation and Employer's Liability

- A. \$100,000 each accident
- B. \$500,000 Disease-Policy limit
- C. \$100,000 Disease-each employee

General Liability

- A. \$2,000,000 General Aggregate
- B. \$2,000,000 Products
- C. \$1,000,000 Personal and Advertising
- D. \$1,000,000 Each Occurrence

Automobile Liability

- A. \$1,000,000 Combined Single Limit

Excess Liability

- A. \$4,000,000 Umbrella Form

Method of Payment: Payment for Construct Chip Seal Surface will be made per accepted square yard in place. The bid price for Constructing Chip Seal Surface shall include all costs associated with furnishing, preparing, hauling, mixing, and applying all materials.


VILLAGE OF
McFarland
SUMMARY SHEET

MEETING DATE: Tuesday, April 13, 2021

SECTION: Business

DEPARTMENT: Public Works

CONTACT: Jim Hessling, Public Works Director, Andrew Bremer, Comm & Eco
Dev Director, Matt Schuenke, Village Administrator

AGENDA ITEM: Update and discussion regarding GIS Development.

PREVIOUS ACTION:

Asset management was discussed at a joint meeting of the Public Utilities and Works Committees at their meeting on February 16, 2021.

ISSUE SUMMARY:

Enclosed within your packet are the goals and objectives for the Public Works Department to advance our GIS System. Our last discussion on this topic was in February where we discussed the use of GIS in terms of managing our public infrastructure. The direction at that time was to prepare a plan outlining our goals and objectives for the development of this system including possible costs, timeline, and work assignments. The enclosed plan incorporates the following goals with summary:

1. **Acquiring new hardware/equipment to support technology needs for ongoing GIS development and capabilities** - There are a number of items suggested that will enhance our ability to collect data and integrate GIS into the field. Not having these things doesn't preclude our ability to advance the use of GIS but it certainly helps accelerate everyone's participation and understand.
2. **Integrate related tasks into daily operations in order to gather real time information for use within the GIS System** - There are many different ways the equipment can be used to gather data for use within the program. It starts with training on the use of the equipment and getting Staff acclimated. There are several items listed where it can be used immediately, and many other opportunities within the Village it could further apply to for data collection.
3. **Build, update, and expand database for GIS Development** - The system itself as it sits here today has many different applications. As we expand and grow our use, this item is important to set standards to help serve as our guide for planning efforts within the next goal.
4. **Utilize GIS System to assist in planning for future developments and public infrastructure needs** - Lastly, it is important that all of the work done to get to this point is put forth for use in a meaningful manner. Setting up the system to be able to aid in the process to manage our public infrastructure and act as a resource for capital improvement planning.



This items are presented in progression of implementation. First we buy the equipment needed, then we start to use that equipment in the field, followed by using the data the field review presents, and incorporating that into our future planning for public infrastructure. This is not a drop everything type of plan nor is it initially suggested to contract for a great deal of these services. Aspects of this work will need to be worked into everyday responsibilities once the technologies are made available for Staff to use. Over time then the system develops and we continue to become more proficient at it will it continue to aid in our management of public infrastructure.

FINANCIAL/BUDGET IMPACT:

We already have incurred the cost of having a GIS system with the data that is presently available. This includes the operation of the software and associated data storage, granted though we haven't tapped into this too far as of yet. Most of those costs are not included within this plan because they already exist as part of our operations and will continue to do so with or without this plan.

The main costs of this plan are up front regarding equipment needed for data collection to be used within software/storage capabilities we already possess. The main items are the GIS Tool at \$6,000 and new mobile computers at \$3,000. The GIS Tool will be new entirely while the computers are a replacement. The purchase of these items can be made through multiple funds within existing budget parameters. Most of the work after that is proposed by Village Staff. We may encounter the desire to hire someone to do some of this work (i.e. - someone contracted to mark all curb stops), but with this being somewhat new it would be best to allow Staff the ability to do this to learn more about how it works. There may be other costs in the future we can consider as the system continues to develop. We can reevaluate these goals/objectives as much as is needed to ensure it is able to assist in our management of the public infrastructure.

VILLAGE PLAN REFERENCE:

None.

ORDINANCE REFERENCE:

None.

BOARD, COMMISSION OR COMMITTEE RECOMMENDATION:

No action is required on this plan as this is mainly a function of the work conducted by Staff to utilize and build the system.

ATTACHMENTS:

1. GIS Development Goals-Objectives 04092021 mgs

McFarland Public Works

GIS Development Plan - Goals and Objectives

Goal:

1	Acquiring new hardware/equipment to support technology needs for ongoing GIS development and capabilities.
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Objective(s):

(A)	GIS Tool				
	Acquisition of GPS reader equipment that is used to mark locations in the field for use within GIS mapping systems. The reader collects the coordinates in the field in real time and adds it to the database for viewing within the GIS system.				
	\$6,000		4-6 weeks		Hessling

(B)	Mobile Computers/Tablets				
	Currently have 4-6 mobile devices beyond their useful life that were repurposed from other uses. Outfitting these within the trucks and assigning to Employees will allow the Employee to use the technology to access data, maps, collection information, etc. while they are working within the field. These are presently not hooked up to the network and have limitations on their use through hot spots. Presently they are mainly used for locating work and water main breaks. Proceeding to purchase 2 computers now that are designed for outdoor use with air cards offering better mobility. Additional computers will be purchased annually thereafter until all have been upgraded and assigned to each employee as utilization expands.				
	\$3,000		4-6 weeks		lgl

(C)	Handheld Garmin Unit				
	GPS device used for locating items by coordinate. Helps to find things in the field that are not attached to an address.				
	\$300		4-6 weeks		lgl

(D)	Additional ArcGIS Licenses				
	We currently have one license for read/write access to our GIS System. This is maintained by the Community and Economic Development Director. The rest of Staff accesses the system in a read only mode. If we want to expand this in the future to have more people that can change or manage the data in addition to reading it, we'll need to acquire more licenses. Will need to assess Staff capabilities to do so and line up associated training to be able to implement this in the future. Future demand for online storage needs and credits may be warranted as expansion of GIS platform develops.				
	\$350 per license		1-2 years		Bremer

McFarland Public Works

GIS Development Plan - Goals and Objectives

Goal:

2	Integrate related tasks in to daily operations in order to gather real time information for use within the GIS system.
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Objective(s):

(A)	Employee Training				
	Employees will need to be trained on how to use the new equipment both to gather data and also on how to use the system in association with their job responsibilities.				
	\$6,000		Ongoing		Hessling

(B)	Curb Stop Locations				
	Utilizing equipment purchased and training received to locate all curb stops in the Village. This will aid in management of water utility when water needs to be shut off between the main and the laterals servicing customers due to main breaks or construction along with wintertime locates/shut offs.				
	TBD		6-12 months		Crew

(C)	Water Valve Locations				
	Utilizing equipment purchased and training received to continue to locate all water valves in the Village. This will aid in management of the water utility to help efficiently identify locations for water valves controlling flow between mains. Also records data such as torque, turns, etc.				
	TBD		Ongoing		Crew

(D)	Sanitary Sewer Inverts				
	Most of the sanitary sewer system operates via gravity and pipes are accessed thru manholes. There is a slight variance in elevation between the incoming and outgoing pipe which is referred to the invert. Including this information helps to understand flow within the system.				
	TBD		12-18 months		Crew

(E)	Sidewalks				
	Sidewalk construction, maintenance, replacement, and other aspects can be managed through the GIS system. Once mapped out, specific areas of improvement can be noted in the system to better track repair and development.				
	TBD		18-24 months		Crew

Opportunities to utilize the equipment as depicted above are endless. This could also apply MS4 Permit responsibilities, easements, Village owned property, park development, sump pump headers, etc. As we become more proficient with the equipment the longer this section grows.

McFarland Public Works

GIS Development Plan - Goals and Objectives

Goal:

3	Build, update, and expand database for GIS Development.
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Objective(s):

(A)	Review Existing Layers			
	There is a lot of information already developed within the system. Like any sort of data management project, the outcome is only as good as the data presented within it. This item would require the existing layers to be reviewed to determine their accuracy and usefulness.			
	\$5,000		Ongoing	Village Engineer C&E Devel Dir.

(B)	Community Development Layers			
	New layers of information can be added to properties to track variances, conditional use permits, planned developments, etc. that are approved.			
	TBD		Ongoing	Planning and Zoning Assistant

(C)	Solar Home Layers			
	The installation of solar onto homes or businesses requires a building permit. This information could use used to track and identify which properties have added solar. This could further aid in its growth and development into the future.			
	TBD		Ongoing	Planning and Zoning Assistant

(D)	Developing Key Attribute Data			
	Each entry into the GIS system can contain an almost endless list of data to accompany the item. Things like year installed, last repaired, material, etc. are all possible for inclusion within the system. Need to review existing data and verify what is it we need for attributes in order to make decisions, then work on getting the data added, and use it to set parameters/queries for future planning.			
	TBD		Ongoing	Planning and Zoning Assistant

McFarland Public Works

GIS Development Plan - Goals and Objectives

Goal:

4	Utilize GIS system to assist in planning for future developments and public infrastructure needs.
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Objective(s):

(A)	Use of the Query Tool				
	The query tool within the GIS software can help you search for various different data points within the system. More review is needed as part of the training to align with future planning needs on what we should be searching for.				
	\$500		Ongoing		All

(B)	Forecasting Infrastructure Needs				
	The inclusion of more key attributes into the system along with the utilization of the query tool will help to forecast future infrastructure needs. Developing these assumptions will be important to framing how the system can help forecast future capital needs.				
	TBD		Ongoing		All

(C)	Set Minimum Parameters				
	Assumptions need to be made as to when something might need to be considered for capital improvement. Like how old is a pipe before we start to think about replacing it and other types of factors. Parameters can be established to help establish benchmarks to guide the query process.				
	TBD		Ongoing		All

(D)	Link to Public Infrastructure Management System				
	All of this work culminates with a better overall management of our public infrastructure and we need to establish a GIS system that is complimentary to how our capital improvements are planned. Whether that's through a similar process as we prepare such projects today or through the inclusion of additional/new technology, the advancement of our GIS capabilities should complement this effort.				
	TBD		Ongoing		All


VILLAGE OF
McFarland
SUMMARY SHEET

MEETING DATE: Tuesday, April 13, 2021

SECTION: Staff Reports

DEPARTMENT: Public Works

CONTACT: Jim Hessling, Public Works Director

AGENDA ITEM: Presentation of the Public Works Monthly Report

PREVIOUS ACTION:

ISSUE SUMMARY:

FINANCIAL/BUDGET IMPACT:

VILLAGE PLAN REFERENCE:

ORDINANCE REFERENCE:

BOARD, COMMISSION OR COMMITTEE RECOMMENDATION:

ATTACHMENTS:

1. March 2021 Public Works Directors report (002)

PUBLIC WORKS COMMITTEE

April 13, 2021

PUBLIC UTILITIES COMMITTEE

April 13, 2021

(joint meeting)

Public Works Directors Report

for

March 2021

The following is information concerning events and activities of the Public Works Department along with the Water and Sewer Utilities for the previous month. This information is provided in brief to provide an overview of the highlights.

PW Complex

Solar panels at the PW facility were activated and are now producing energy.

Salt Training

Public works employees attend a salt training workshop. The class spoke to salt usage, calibration of equipment and methods to improve delivery. All participants from that day's event, passed their testing requirements and are now certified salt applicators.

Tree Trimming

We were again trimming various trees along the streets and in the parks during the month of March.

Vacancy

The department has an open position due to the announced departure of Spencer Warrenburg. We thank Spencer for his aid to the department.

New Hire

The Department of Public Works is pleased to announce Kean Paltz as the newest Public Works employee to join us. Kean comes to us from the U.S. Air Force. Welcome Kean.

Compost/Brush Site

The compost/brush site was opened earlier than usual due to the warmer temperatures that we experienced in March.

Bucket Truck

The replacement bucket truck arrived in March. Employees were trained in its operation.

Snow

Staff did not work on any snow events in March. The quick moving storm of March 15 left approximately 2 inches of snow. Temperatures in the mid 40's the next day melted it all.

Community Assistance with Interviews

Hessling participated in the recruitment selection process for a new water system supervisor for the City of Fitchburg.

Street Sweeping

Street sweeping activities started in March. The street sweeper is expected to be used throughout the summer and fall.

Meetings/Training/Seminars

All meetings were held by electronic means this month. Those meetings include:

- Road Salt 101 for Wisconsin Towns and Villages - Igl, Irwin & Hessling
- I/I meeting with MMSD - Hessling
- Diggers Hotline Utility Damage Prevention - Brandt, Weber, Krey
- Salt Wise training - Warren, Weber, Warrenburg, Kelln, Pospyhalla, Kelln, Krey, McDade, Larson, Igl, Irwin & Hessling
- Park Planning in an Era of Recreational Trends and Challenges - Larson
- Cooperative Purchasing Lunch N Learn - Igl
- Your Route to Smooth Pavement: Rate, Plan Budget - Irwin, Igl & Hessling
- APWA Young Professionals meeting - Irwin
- Conflict Management in the Workplace - Irwin
- Delinquent and Shut Off Notices - Irwin
- Establishing Inclusion as a Value - Irwin & Igl
- SCRUFFS meeting (new oak wilt tool) - Larson
- NPWW in the Virtual World and Other Outreach Conversations - Hessling
- Sourcewell Purchasing - Igl
- New funding for WI Street projects - Transportation Utilities - Igl
- MSA Park Planning Seminar - Larson
- Fruit tree webinar – Larson
- PHMDC – monthly update/guidance on new orders - Larson
- APWA monthly board meeting - Igl & Hessling
- Hessling attended the following EPA Training Events
 - AWIA Emergency Response Plan Template and Guidance
 - AWIA Risk and Resilience Assessments: Malevolent Acts, Natural Hazards, and the Small Systems Checklist
 - AWIA Emergency Response Plan Template and Guidance

2021 WATER SYSTEM IMPACT FEES

Collected in Month	2021 Fees	2020 Fees	2021 Impact Fee Distribution		
			Tower	Main	Well
January	-	1,950.00	-	-	-
February	5,851.00	4,550.00	3,298.86	936.16	1,615.98
March	3,900.00	4,550.00	2,198.88	624.00	1,077.12
1st Quarter Total	9,751.00	11,050.00	5,497.74	1,560.16	2,693.10
April	-	10,402.00	-	-	-
May	-	1,950.00	-	-	-
June	-	3,250.00	-	-	-
2nd Quarter Total	-	15,602.00	-	-	-
July	-	3,900.00	-	-	-
August	-	2,600.00	-	-	-
September	-	1,950.00	-	-	-
3rd Quarter Total	-	8,450.00	-	-	-
October	-	1,950.00	-	-	-
November	-	9,101.00	-	-	-
December	-	18,701.16	-	-	-
4th Quarter Total	-	29,752.16	-	-	-

HISTORICAL WATER IMPACT FEE TOTALS

2021 Total	9,751.00		5,497.74	1,560.16	2,693.10
2020 Total	64,854.16		38,222.36	10,020.80	16,611.00
2019 Total	57,201.00		32,250.79	9,152.16	15,798.05
2018 Total	71,501.00		40,313.34	11,440.16	19,747.50
2017 Total	60,801.20		34,281.17	9,728.00	16,792.03
2016 Total	38,026.00		23,708.24	5,252.00	9,065.76
2015 Total	5,851.00		3,298.92	936.00	1,616.08
2014 Total	7,150.00		4,031.28	1,144.00	1,974.72
2013 Total	21,125.00		11,910.59	3,380.00	5,834.41
2012 Total	13,650.00		7,696.08	2,184.00	3,769.92
2011 Total	12,350.00		6,963.12	1,976.00	3,410.88
2010 Total	5,200.00		2,931.84	832.00	1,436.16
2009 Total	7,150.00		4,031.26	1,144.00	1,974.74
2008 Total	10,400.00		5,863.62	1,664.00	2,872.38
2007 Total	34,451.00		19,423.88	5,512.16	9,514.96
2006 Total	28,927.00		16,309.33	4,628.32	7,989.35
2005 Total	52,326.00		29,501.92	8,372.16	14,451.92
2004 Total	77,679.00		43,796.20	12,428.64	21,454.16
2003 Total	59,802.00		33,716.97	9,568.32	16,516.71
2002 Total	69,625.00		39,255.27	11,140.00	19,229.73
2001 Total	55,271.50		31,162.62	8,843.44	15,265.44
2000 Total	56,701.00		31,968.59	9,072.16	15,660.25
1999 Total	55,388.00		31,228.31	8,862.08	15,297.61
1998 Total	14,581.73		8,221.33	2,333.08	4,027.32
Grand Total	\$ 815,157.43	\$ -	\$ 461,864.67	\$ 129,592.68	\$ 223,700.08

\$650=	\$366.48	\$104.00	\$179.52
\$1300	\$732.96	\$208.00	\$359.04

Tower= .56381, Main=.16, Well=.27619